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# DOWNTOWN MASTER PLAN

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BOILING SPRINGS | NORTH CAROLINA  
SEPTEMBER 2020

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DOWNTOWN MASTER PLAN

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BOILING SPRINGS | NORTH CAROLINA

DOWNTOWN MASTER PLAN  
VISION & DESIGN GUIDELINES

PREPARED FOR:  
TOWN OF BOILING SPRINGS | NC

PREPARED BY:  
COLEJENEST & STONE, P.A.  
ALTA PLANNING + DESIGN

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## DOWNTOWN MASTER PLAN

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BOILING SPRINGS | NORTH CAROLINA

### INTRODUCTION

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### ANALYSIS

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### CORRIDOR STUDY

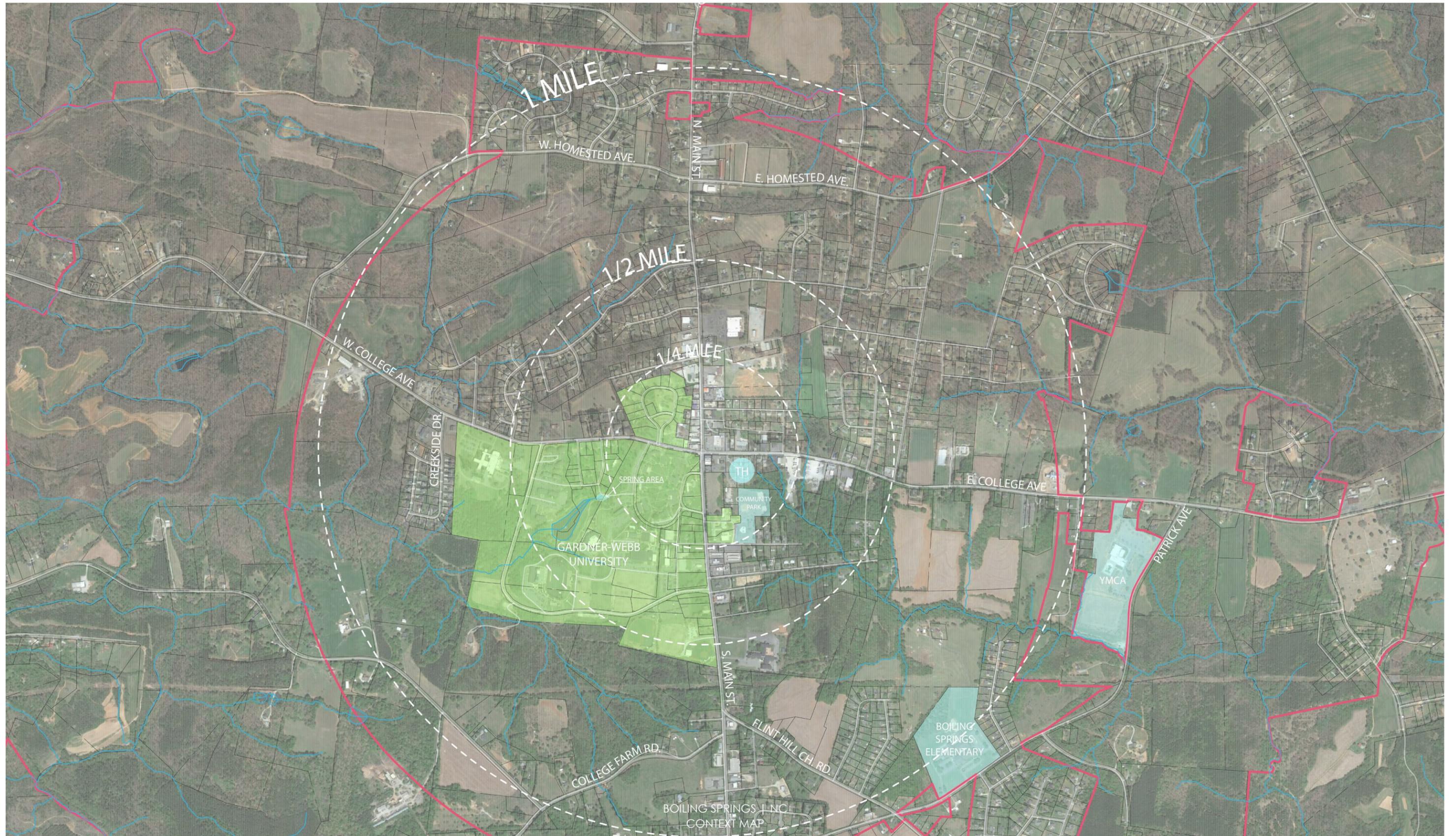
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### RECOMMENDATIONS

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# INTRODUCTION

BOILING SPRINGS | NC

## Context:

The Town of Boiling Springs is located 50 miles west of Charlotte, NC and 30 miles east of Spartanburg, SC in the Piedmont area of western North Carolina. It is in Cleveland County and is approximately ten (10) miles southwest of the City of Shelby, the county seat. According to the U.S. Census Bureau, the 2016 population estimate based on the 2010 census for Boiling Springs is about 4,819, a growth of about fifty (50) percent since 1990. Gardner-Webb University, originally a Baptist boarding school, currently has 1,100 on-campus students and staff.

Boiling Springs is served by US-74, NC-150, and other state roads. The Town's proximity and accessibility to interstate I-85 provide residents with a quick and easy route to three (3) major metropolitan areas, Charlotte, Spartanburg, and Greenville, and is conveniently located around other regional destinations. The Town is divided into four (4) roughly equal quadrants. The main east/west connector is College Avenue, while the main north/south connector is Main Street.

Since Boiling Springs incorporation in 1911, the Town has maintained its commitment to the lure and attraction of the "small town feel" the area offers. With no large industrial entities or corporations, Boiling Springs has been able to preserve the small town charm that appeals to the local residents. The Town's limited commercial development provides both advantages and disadvantages in terms of proximity to local goods and services and its tax base.



GARDNER-WEBB UNIVERSITY CAMPUS



BOILING SPRINGS STREETSCAPE



BOILING SPRINGS AERIAL, 2019

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## PURPOSE AND GOALS

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BOILING SPRINGS | NC

### Purpose of Downtown Master Plan:

The Boiling Springs, NC Downtown Master Plan was initiated by the Town Manager and Town Council. The Master Plan's vision is to adopt a framework that allows the Town to grow respectfully and strengthen its identity while informing future Zoning Ordinance and Land Use Plan decisions. Based on the development trends of the last 30 years, the Town of Boiling Springs has an abundance of automobile dependent streets, shops, and public facilities that have created an unwelcoming downtown core. In order to revitalize the downtown area, the Town of Boiling Springs, in conjunction with local residents and key community leaders, has undertaken an effort to provide additional design guidelines within the 1/4 mile & 1/2 mile radius of the core intersection of College Avenue & Main Street. These recommendations will support pedestrian-oriented streetscapes, prioritize pivotal pilot projects that alleviate broken connections, build a brand/identity, and promote shops and small businesses that define the "small town" feel by creating places with strong cultural identities.

### Goals of Downtown Master Plan:

- 1) **Land Use** - Establish a "Downtown District" that is unique to the existing business corridor overlay. This district will allow for flexible building types, including a number of additional housing types and small businesses that create a pedestrian-oriented downtown area. First floor retail uses shall be encouraged, as additional shopping and dining opportunities will enhance neighborhood connectivity.
- 2) **Transportation / Parking** - Highlight short- and long- term parking opportunities by evaluating intermittent street improvements that stimulate rear-oriented, inner-connecting parking facilities. Blending a system of roads that reduce higher speeds while providing pedestrian and cyclist with widened fields of travel, will encourage a more dynamic and spirited downtown area.
- 3) **Economic Development** - Endorse development and redevelopment opportunities within the existing framework. Maximizing return on investment in small downtown areas requires modest goals that include supporting small businesses, revitalizing public functions, creating outdoor space that bolsters community participation, and bridging economic and social challenges. Longer term aspirations will include additional medical and public services, medium- to high- density residential, and mixed-use office and retail buildings.
- 4) **Parks and Recreation** - Provide access for residents to connect with public facilities by supporting pivotal relationships to existing private and public institutions.
- 5) **Planning + Zoning Documents** - Consistently updating a town's Code of Ordinances benefits both the local and development communities. By adopting this Master Plan document, the Town's commitment to implementing the strategies noted can seem ineffective without the proper legal authority to achieve the desired goals. Update the Future Land Use Map, enhance specific development standards within the code, update the current zoning map, and implement the Park and Recreation Master Plan.

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## TOWN'S ROLE

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BOILING SPRINGS | NC

### Town's Role in Future Development

- **Town Catalyst:** The Town's primary role is to promote economic development, respond to citizen demands, bolster relationships, and encourage respectful growth. Additional incentives for flexible zoning, future land use plan updates, economic incentives through bond structures.
- **Public Investment Proceeds Private Development:** Coordination of common infrastructure and utilizing public/private facilities will spur additional development. Creating a unique streetscape that includes critical elements such as signage, street lighting, eating areas, crosswalks, and supplemental landscaping will bolster the "small town feel".
- **Public Event Programing:** Increasing the Town's public awareness will encourage public event spaces including amphitheaters, museums, garden spaces, open activity lawns, and indoor performance centers. Additional incentives, local and state grants, and greenway/streetscape bonds can assist in generating additional funding and provide enough flexibility for future growth.
- **Guidelines for Implementation:** This document outlines the basic structure that will enhance the downtown area within Boiling Springs' core. The introduction of the Downtown District will allow redevelopment and new development opportunities to create larger building envelopes, which encourages builders to take advantage of lesser setback restrictions. Although build-to-lines will encourage street-oriented buildings, ensuring parking facilities are required to be located along the rear of the building where possible is critical. As with other municipal Master Plan documents, this outline provides the tools and enhancement opportunities that will add immediate and lasting impacts.
- **Maintain relationships with Experts:** Regularly updating the future land use and zoning maps and ordinances will benefit both the public institutions and local neighbors.

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# BOILING SPRINGS | NC ANALYSIS

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Town History

Gardner-Webb History

Existing Conditions - Open Space

Existing Conditions - Character

Existing Circulation

Existing Zoning + Land Use

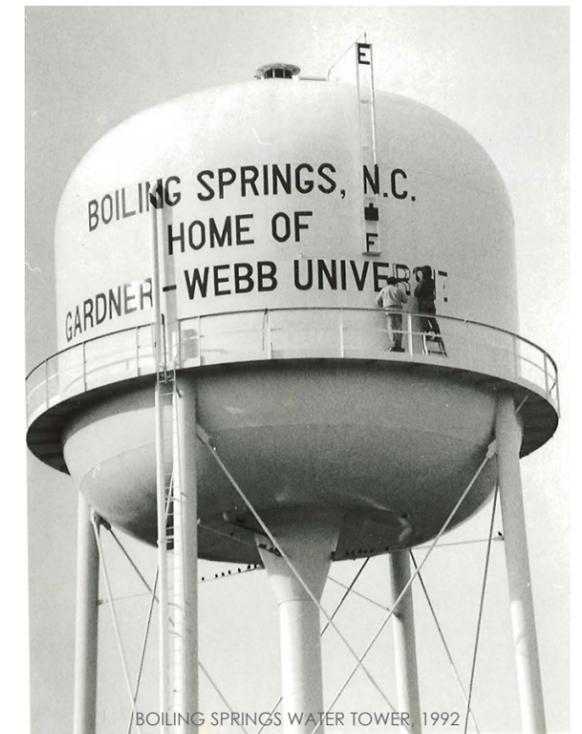
## TOWN HISTORY

BOILING SPRINGS | NC

The Town of Boiling Springs, NC is named after two underground springs that measured approximately five feet across and bubbled six to eight inches above the surface. Settlers were attracted to the area because of this reliable water source. Just outside the current town boundaries, there were many small farms built. One example is the Irvin-Hamrick home built in 1795 as shown on the left page. The first families to settle the now Boiling Springs area were the Hamricks, the Greenes and the McSwains in 1843. One of the first buildings to be built was the Baptist Church in 1847 which was built only 100 yards from the springs. In the latter part of the 19th century, the settlement of Boiling Springs was known as a “bedroom community,” with no railroads, no industries, few stores, and no paved streets.

In the 20th century, things began to change. Boiling Springs High School was founded and opened in 1905. In 1911, the town was incorporated with 250 residents. With the influence of the town’s school authorities, Senator O. Max Gardner introduced the bill to incorporate the Town and the new limits were decided by drawing a mile and a half radius from the school’s original bell tower. Remnants of the original boundary can be seen in the current town boundary (See Area map on Page 10). The incorporation of Boiling Springs proved to be a major step forward because it allowed Boiling Springs to grow and it provided a government to make improvements to public infrastructure. With the infrastructure in place the town could support the growth of the college and in response, the growth of the town. The Town of Boiling Springs has had steady population growth in the decades following World War II.

As the small town feel and charm became the livelihood and identity of the Town, growth steadily increased throughout the mid- to late- 20th century, including massive growth (roughly 50% growth) from the 1960s through the 1970s, and again in 1990 through the turn of the century. Yet since 2010, the population for the town has not experienced the same growth as other surrounding cities/towns. The population has remained relatively constant since, and while the surrounding real-estate market dramatically recovered after the 2008 housing bubble crash, Boiling Springs did not see an influx in new development until recently.



## GARDNER-WEBB HISTORY

BOILING SPRINGS | NC

Because Boiling Springs was founded as a Baptist Community, the citizens were very interested in Christian education. In the beginning of the 20th Century, the Kings Mountain Baptist and Sandy Run Associations chose Boiling Springs to build their new high school. They said they chose the town "for its beauty, for the general thrift of its people and for the high morale and religious tone of its citizenship." Boiling Spring citizens were very supportive of the idea and promised more money and building materials than any other community being considered by these associations.

In 1905, the Boiling Springs High School was opened as a boarding institution. Not long after, the school was very influential in the Town's decision to become incorporated. The school authorities didn't want cigarettes or "intoxicating drinks" to be sold near the school so they appealed to the legislature to pass a bill to incorporate Boiling Springs into a Town. The bill was introduced by Senator O. Max Gardner in 1911. The original school building had a bell tower that served as the center point of the one and a half mile radius circle for the newly incorporated town.

In response to the decreased enrollment because of the competition from public high schools, Boiling Springs High School became Boiling Springs Junior College in 1928. The institution survived the Great Depression with generous donations from loyal supporters. One such supporter was Elijah Bly Hamrick. In 1937, when the historic high school hall, that was built in 1925, became devastated by fire, they rebuilt the hall and renamed it, E.B. Hamrick Hall, in honor of his generous support. During World War II, Governor O. Max Gardner began devoting his energy, time and wealth to strengthening and guiding the College. He referred to Boiling Springs Junior College as the school that "refused to die." In 1942, the college was renamed Gardner-Webb Junior College in honor of the governor, his wife Fay Webb Gardner, and their families.

Gardner-Webb Junior College began to grow the decades after World War II and developed into a four-year senior college in 1971. Gardner-Webb's continued growth allowed it to develop into a University in January 1993.

Today, Gardner-Webb University remains a major part of the Town identity, economy, history, and future.



GARDNER-WEBB MAIN/COLLEGE MONUMENTATION, 2019



GARDNER-WEBB CAMPUS EVENT, 2019



BOILING SPRINGS JUNIOR COLLEGE, 1928



GARDNER-WEBB UNIVERSITY CAMPUS, 2019



HUGGINS CURTIS OPENS, BOILING SPRINGS HIGH SCHOOL, 1907



## EXISTING CONDITIONS- OPEN SPACE

BOILING SPRINGS | NC

The Town of Boiling Springs has abundant existing open space. Not only does it have 300 acres of campus open space and a public park, it is surrounded by natural undeveloped land characterized by hardwood forests, pine forests, and open fields.

**Campus** - Gardner-Webb University is 300 acres located in the southwest quadrant. Not only does the campus have naturalized spaces in relation to Lake Hollifield, the central pond on campus located between Tucker Student Center, student housing and the Football Stadium, it also has more formal campus open space such as "The Quad" which is a popular lawn located at the center of campus and is close to the intersection of Main Street and College Avenue. The University has the luxury of sufficient space for new buildings giving the campus a more open, relaxed aesthetic.

**Public Parks** - There is one (1) formal park in the Town of Boiling Springs. This is located in the southeast quadrant and is behind Town Hall. A project was done for the Town which focused on improvements to this park. (See Park Plans in Appendix) Within the park and Town Hall's proximity is the local cemetery which provides much-needed greenspace along the Main Street corridor.

**Natural Open Space** - There are three categories of Natural Open Space shown: open fields or vacant lots, Hardwood Forests, and Pine Forests. (see left map) There are some vacant lots in the north quadrants which are great opportunities for development due to their proximity to the Town center. The hardwood and pine forests are abundant and are great opportunities for preservation.

**Broad River Greenway** - About 3.3 miles to the south of Boiling Springs is the Broad River Greenway which is a 1,500-acre greenway running along both sides of the Broad River. Cleveland County and the Town of Boiling Springs own it jointly. This site is great for fishing, hiking, biking, walking trails, picnicking, horseback riding, canoeing, and tubing. The greenway also connects to the Carolina Thread Trail which is a regional network of connected greenways, trails, and blueways. This regional trail system includes 15 counties in North and South Carolina.



CAMPUS



HARDWOOD



OPEN



PINE

## EXISTING CONDITIONS - CHARACTER

BOILING SPRINGS | NC

The Town of Boiling Springs' character and identity are deeply rooted in being a small town. Maintaining the "small town feel", even with projected growth, is the most important element in improving the perception of the Downtown core. The ideal vision of the Town of Boiling Springs is to maintain its signature charm by invigorating the local community to awaken a sleepy town with lively stores, outdoor dining, beautiful homes, and a growing university campus.

Because of the town's current size, it has an opportunity to preserve and enhance the current character while still planning for future growth and development. Current land-use (as shown right) has three main uses: Institutional, Commercial, and Residential.

**Institutional** - A major part of the character and identity of Boiling Springs is Gardner-Webb University. Many of the buildings on the campus are historic, unique or important to the community. Other institutional land-uses are government buildings and churches. Stressing the importance of the local community's connections with the College's students and staff is central to harboring a strong relationship between the Town and Gardner-Webb.

**Commercial** - There are two main types of commercial uses in the town. Commercial 1 is characterized by building oriented fronting the street. Commercial 2 is characterized by buildings off the street with a bay of parking immediately adjacent to the front entry. Commercial 1 is the preferred building orientation, since it adds more to personality and appeal to the immediate streetscape, while Commercial 2 can dilute it and make a place seem more generic and feel like "any other place." An emphasis on Commercial 1 type storefront and multi-use development will be vital in activating local businesses and reinforcing a walkable downtown.

**Residential** - The majority of surrounding housing is single-family residential (low-density housing). There are very few multi-family residential areas (medium-density housing), and where moderate density housing exists, it is generally limited to student housing for Gardner-Webb. The current housing inventory includes a variety of housing styles and types, but most commonly utilized are the ranch-style brick homes and mid-century homes. The surrounding residential areas need more variety of housing types with increased density. Administering an assortment of housing types is crucial to the long-term diversity and local economic sustainability of the local housing market.



INSTITUTIONAL



COMMERCIAL 1



COMMERCIAL 2



RESIDENTIAL

## EXISTING CIRCULATION

BOILING SPRINGS | NC

**Pedestrian Circulation** - Many improvements have been made to the Town's pedestrian circulation network in recent years, however, there are areas that are still lacking connectivity. Due to the lower scaled density adjacent to the Town core, Gardner-Webb's University campus, and proximity to the Broad River Greenway, Boiling Springs has the unique opportunity to create a comprehensive pedestrian network that thrives. The October 2006 Pedestrian plan, determined that sidewalk improvements along College Avenue and Main Street were critical. The plan also proposed additional pedestrian paths throughout to encourage the construction of active trails. Notable improvements were focused near the University and along the south edge of the Boiling Springs business corridor. These improvements established sidewalks on both sides of the street at the town core. However, power poles are impeding circulation. Utility conflicts are constant and highly visible, and a lack of planting strips and street trees hinder attracting pedestrians while creating a less than desirable streetscape experience.

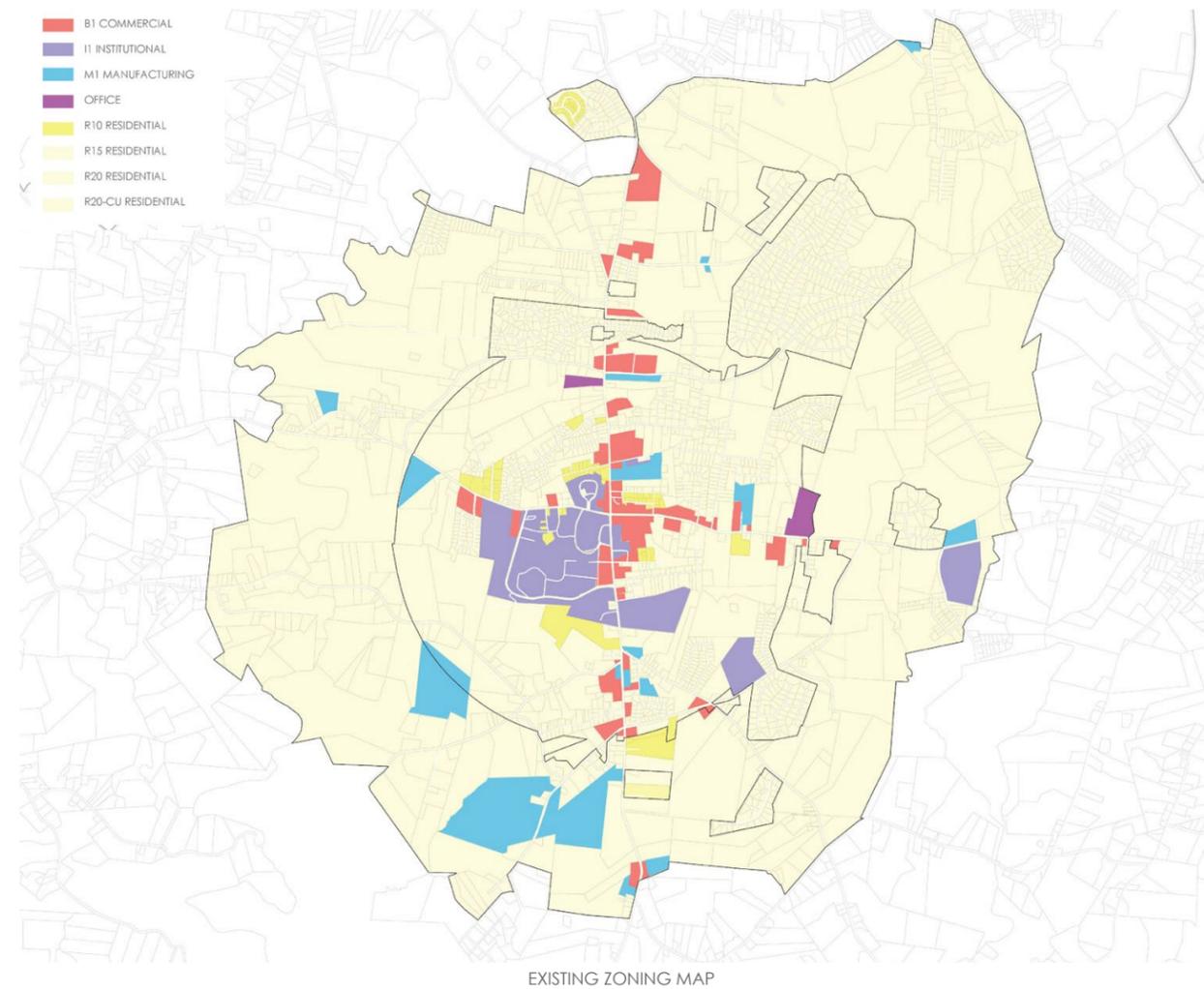
**Vehicular Circulation** - The two main arterials are College Avenue (East/West) and Main Street (North/South). The Town core is located where these two roads intersect. NC 150 travels along East College Avenue and turns on South Main Street. The other smaller arterials are Patrick Avenue, Flint Hill Church Road, Hillcrest Street, College Farm Road, and Homestead Avenue. The October 2006 Pedestrian plan identified the existing Patrick Avenue (State Road 1149) as an opportunity to create a bypass from East College Avenue to South Main Street to alleviate traffic in the Town core. All other streets are collector and local neighborhood streets. With the growth of the surrounding Charlotte, Greenville, and Spartanburg's Metropolitan area, comes increased traffic volume that often affects smaller communities. Correspondingly, reinforcing positive block structure, instituting more on-street parking, creating strong cyclist/vehicle relationships, and encouraging slower traffic speeds within the Town's core area is vitally important to current and future traffic circulation.



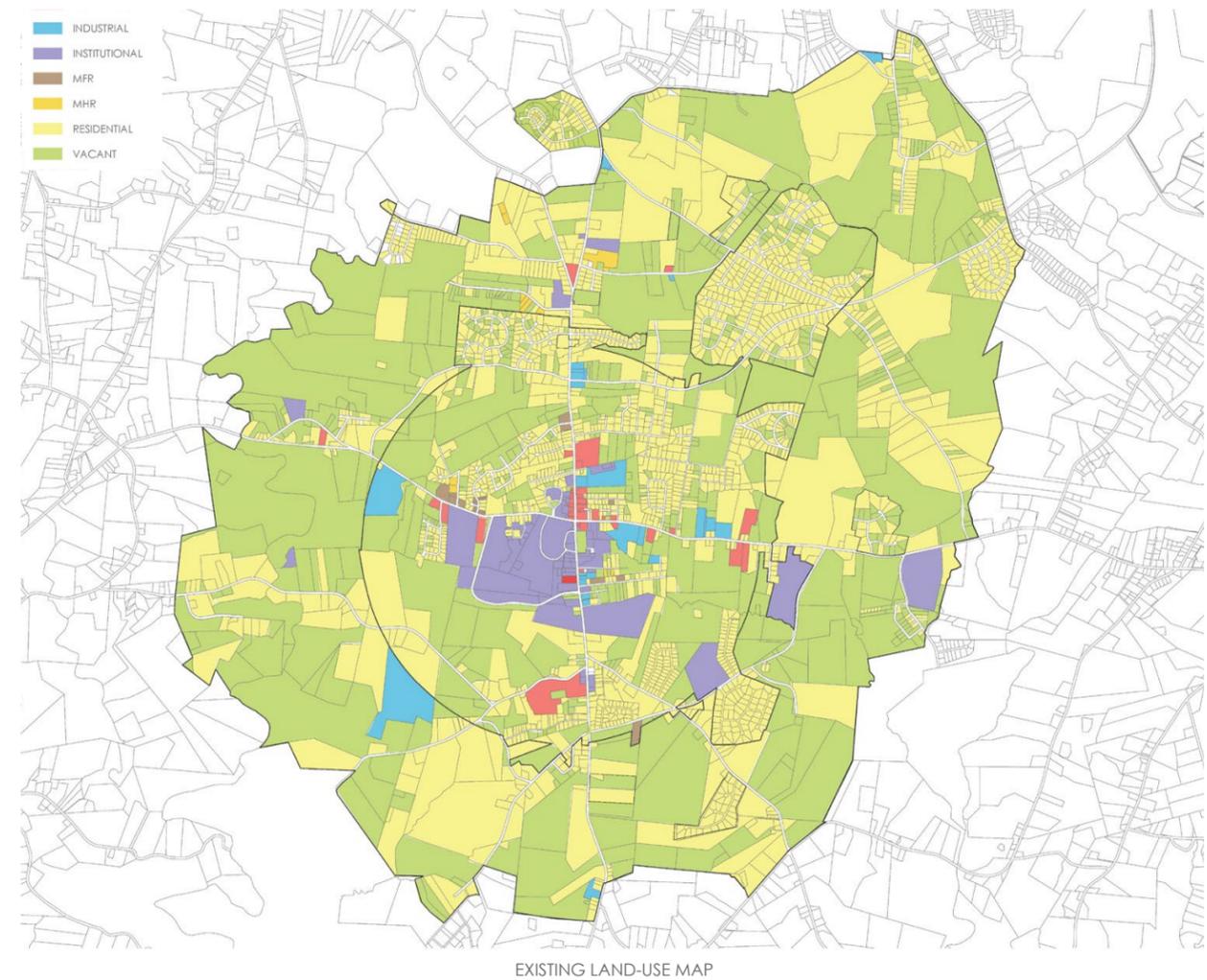
## EXISTING ZONING + LAND-USE

BOILING SPRINGS | NC

The existing Zoning Map for the Town shows most of the land surrounding the downtown core as single family residential (R15, R20, and R20-CU). There are more designated areas of R15 & R20 Residential (low-density) than any other housing zoning classification. Additionally, Institutional and Commercial zoning classifications are centrally located to the Town's core intersection. There are more concentrated housing facilities in areas that are in close proximity to institutional uses such as the University and commercial uses. Commercial uses are focused on the two main roads, Main Street and College Avenue. Additionally, some light manufacturing and office facilities exist near the downtown core, but are limited.



A noticeable residential housing vacancy exists within areas surrounding the downtown core. The majority of low-density housing is concentrated along the outskirts of the Town's limits. With Gardner-Webb's campus controlling significant portions of the Town's core area, a constant influx of younger demographics increase the need for medium- to high- density near the University's campus, which is conveniently located within the downtown area. Subsequently, no mixed-use buildings exist within the Town limits, which can offer dining, residential, retail and office functions. Also, a few light industrial buildings exist on Main Street and College Avenue, but should relocate long-term to locations that encourage more vehicular oriented traffic. No office campus or office parks exists nearby.



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BOILING SPRINGS | NC  
DOWNTOWN MASTER PLAN

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Approach

Public Input Process

Conceptual Master Plan

Priority Projects

Policy + Management & Operation Strategies

Design + Implementation

## APPROACH

BOILING SPRINGS | NC

The Boiling Springs | NC Downtown Master Plan was created in response to the economic opportunity and growth of the surrounding areas. Initiated by the Town Manager and Town Council, the Master Plan's vision is to adopt a framework that allows the Town to grow and strengthen its identity while informing future Zoning Ordinance and Land Use Plan decisions. The Master Plan took shape over several months following an intensive two-day charrette, where individuals were tasked with examining the collective responses and preparing a draft of an adoptable cohesive vision plan.

The intent of the Master Plan is to create a framework plan to strengthen the Town's branding and identity. A defined boundary was established to create a "Downtown District" that would encourage pedestrian-oriented streets and buildings that attract more residential development and spur local commercial activity. This district would set additional standards that promote scalable development, encourage parking connectivity, increased access to public facilities, and improve bike and pedestrian circulation.

After thorough analysis and review, the design team's objective was to formulate a short-term approach based on high-impact implementations that could generate immediate impacts for the local community. Expandable streetscapes and building renovations are key elements to generating momentum, but true advancement will occur when an introduction of new and varied housing typologies and retail adjacent to streets start to spawn new energy and promote exterior investment.

Successful and scalable development occurs over a lengthy period of time, and as such, the design team developed a long-term approach that targeted specific areas that would reinforce the growth and progress of the downtown area over time. By establishing guidelines that generate additional energy harnessed from short-term planning efforts, these long-term planning methods and implementation strategies hope to boost local and regional investment, improve circulation, and liven the downtown area.

This document serves as the synopsis for the short- and long- term development within the downtown core. Boiling Springs prides itself on a strong sense of community that has been maintained over the years. The vision and planning efforts include sensible design impacts to reinforce the Town's identity. By engaging the public during the decision making process, a community voice is commonly represented through every stage of planning. These collaborative efforts have culminated in prioritizing targeted projects that will generate immediate momentum.

Reassuring successful interventions includes key design and implementation strategies, along with careful considerations for phasing. The following pages describe essential decision-making opportunities that can generate a livened downtown scene, both short- and long- term.



PUBLIC MEETING + PRESENTATION (JUNE 2019)



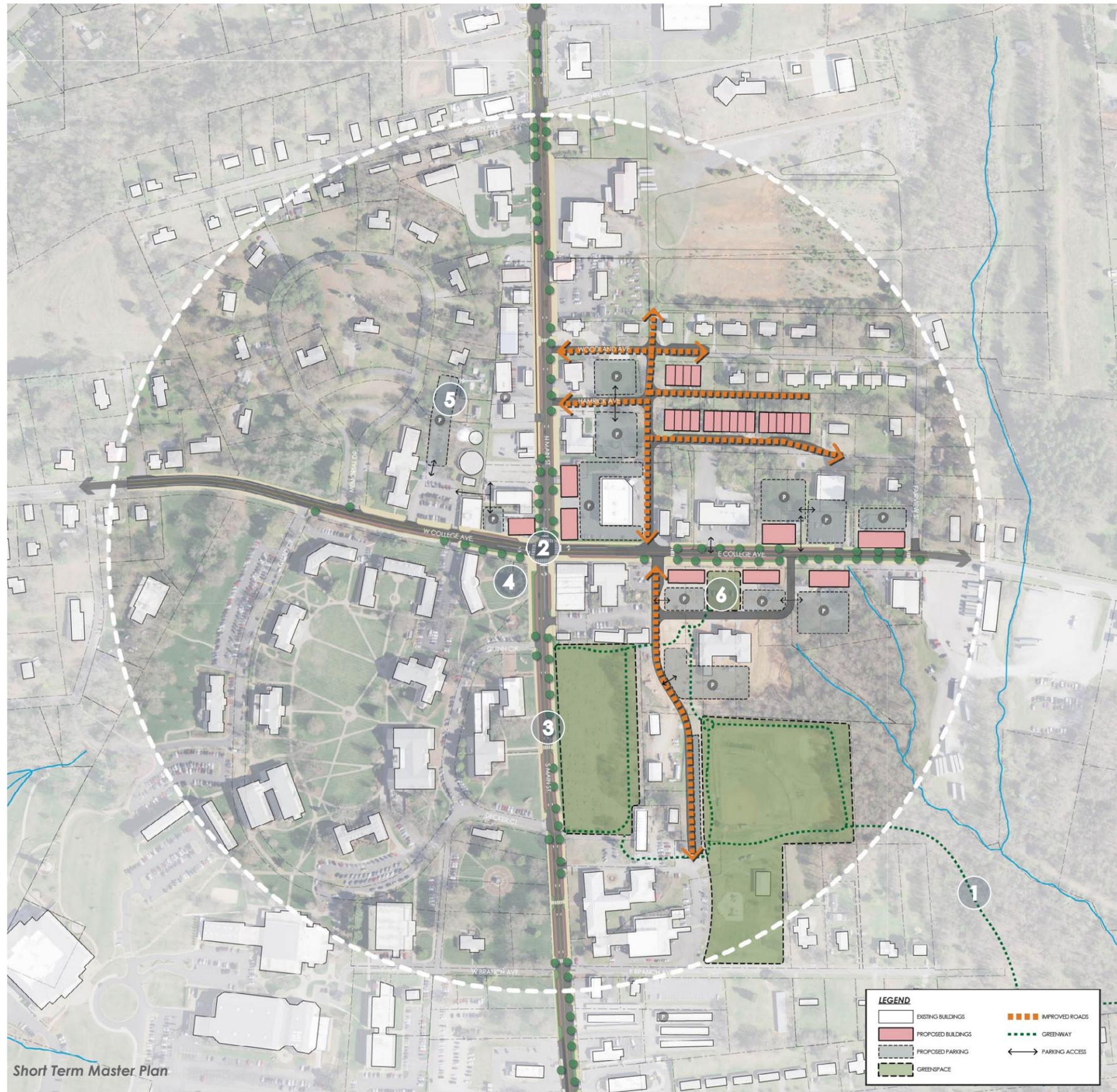
CHARRETTE WORKSTATION SESSIONS (JULY 2019)





# CONCEPTUAL MASTER PLAN

BOILING SPRINGS | NC



## Primary Short Term Strategies:

1) **Greenway to YMCA** - Identified as a critical local greenway connector, the YMCA has been described by local residents as a 'defacto' senior center, which could benefit from a direct connection to the downtown area. Providing a dedicated multi-use path that allows users of all ages to enjoy nature, exercise, and reduce short vehicular trips will increase community awareness and improve walkability.

### 2) Intersection of Main/College Improvements -

**A)** Post Office: An aging public service facility that needs improved pedestrian circulation, parking access, and a revamped building facade.

**B)** Liner Retail at the northeast corner of Main/College: The existing CVS parking lot is currently underutilized, and does not maintain any storefront street presence. Introducing liner retail that includes stores, restaurants, and other commercial services will activate the street, and help establish a firm identity at the main core area.

**C)** Town Green at Southwest Corner: A key opportunity to create a significant public community multi-use space or gateway that highlights the beauty of the University's campus and its identity. With the grade change and large existing vegetation, a shallow amphitheater or programmable event space can connect students and residents.

### 3) Streetscape Improvements along SE Quadrant at Main/College -

**A)** Cemetery Edge: The existing cemetery allows for a centralized green space along Main Street, immediately adjacent from the University. Due to inadequate sidewalk widths and lack of maintenance including lighting, signage, fencing, and landscaping, the cemetery edge is currently underutilized in such a highly visible portion of Town. Minor community improvements include additional walking paths (that can connect to the Town park and future greenway connection), introducing shade trees, creating a mid-block crossing to the University, and improving general functionality and aesthetics.

**B)** South Main St Streetscape: Reducing the existing travel lanes, increasing on-street parking, expanding sidewalk widths, providing traffic calming devices, and adding shade trees will help enhance and promote a walkable and vibrant streetscape that will encourage outdoor dining and storefront activation.

4) **Branding and Identity** - Improve and introduce branding, signage, wayfinding and collaborative arts and visioning programs in conjunction with the University's campus, including the Water Tower.

5) **Shared Parking** - Finding strategic locations to introduce more public/shared parking opportunities. The Water Tower facility currently includes land that is underutilized and could be transitioned to public parking.

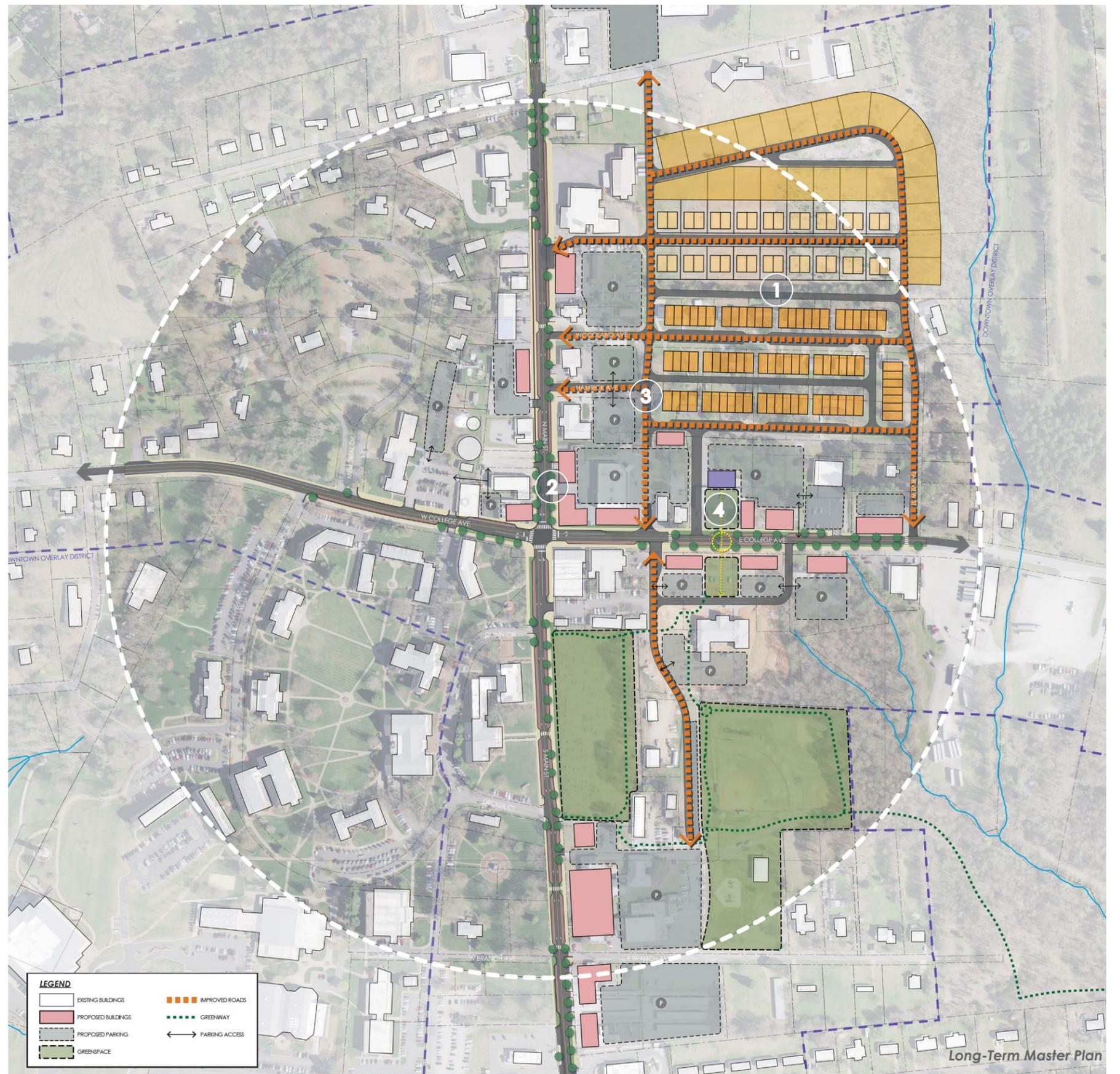
6) **Town Hall Green** - Creating a unique space that allows for programmable events including farmers' markets, pop-up shops and potential festivals will encourage small shop, and dining opportunities.

# CONCEPTUAL MASTER PLAN

BOILING SPRINGS | NC

## Primary Long-Term Strategies:

- 1) **Introduce/Increase Mix of Housing Types** - Communities function best when a mix of housing types are available. An introduction of more medium- to high- density housing is imperative in creating an active core, as well as including affordable housing options for all economic groups. Townhomes, duplexes, and apartment complexes allow residents to more actively engage with the surrounding activities. The current lack of opportunities for-rent or for-sale attached single-family housing typologies (townhomes, condominiums, apartments, etc) can dampen growth and momentum as the Town grows.
- 2) **Build to Lines** - Introducing Build to Lines to the Downtown Overlay District will allow for development to occur closer to the primary street, better engaging the pedestrian/building relationship. Improving sidewalks, parking accessibility, storefront access, and circulation. Prioritizing specific intersections and segments of road improvements with the appropriate agencies is critical to successful integration.
- 3) **Improve Vehicular Circulation & Block Structure** - Introducing more secondary road connections and reducing block length will promote walkability and reduce traffic flow. In the Northeast quadrant of the Main/College intersection provides the ideal redevelopment opportunity to introduce local road connectors. Hamrick Ave and Woodland Ave both present opportunities to integrate "complete streets."
- 4) **Civic Green** - Creating civic space that allows for flexible event opportunities and publicizes the Town's commitment to recreation and open space. Surrounding the Town Hall Green with commercial uses will help support the viability of key event space. Creating an additional Civic building and green space could generate new energy including local farmers' markets, food truck rallies and other Town sponsored events.



# PRIORITY PROJECTS

BOILING SPRINGS | NC

THE PRIORITY PROJECTS LISTED BELOW REPRESENT IMMEDIATE OPPORTUNITIES THAT WILL ENHANCE THE TOWN'S IDENTITY WHILE CREATING MOMENTUM FOR FUTURE DEVELOPMENT. THE SITES DESCRIBED BELOW HAVE PREVIOUSLY BEEN ANALYZED AND DETERMINED TO BE HIGHLY CRITICAL ITEMS TO SPUR COMMUNITY ENGAGEMENT AND ACTIVITY.

## SHARED PARKING

- Water Tower location for temporary parking

## LINEAR RETAIL

- Reduce large parking field at CVS
- Add storefront + dining opportunities

## POST OFFICE RENOVATION

- Revitalized facade and storefront

## CAMPUS/TOWN GREEN

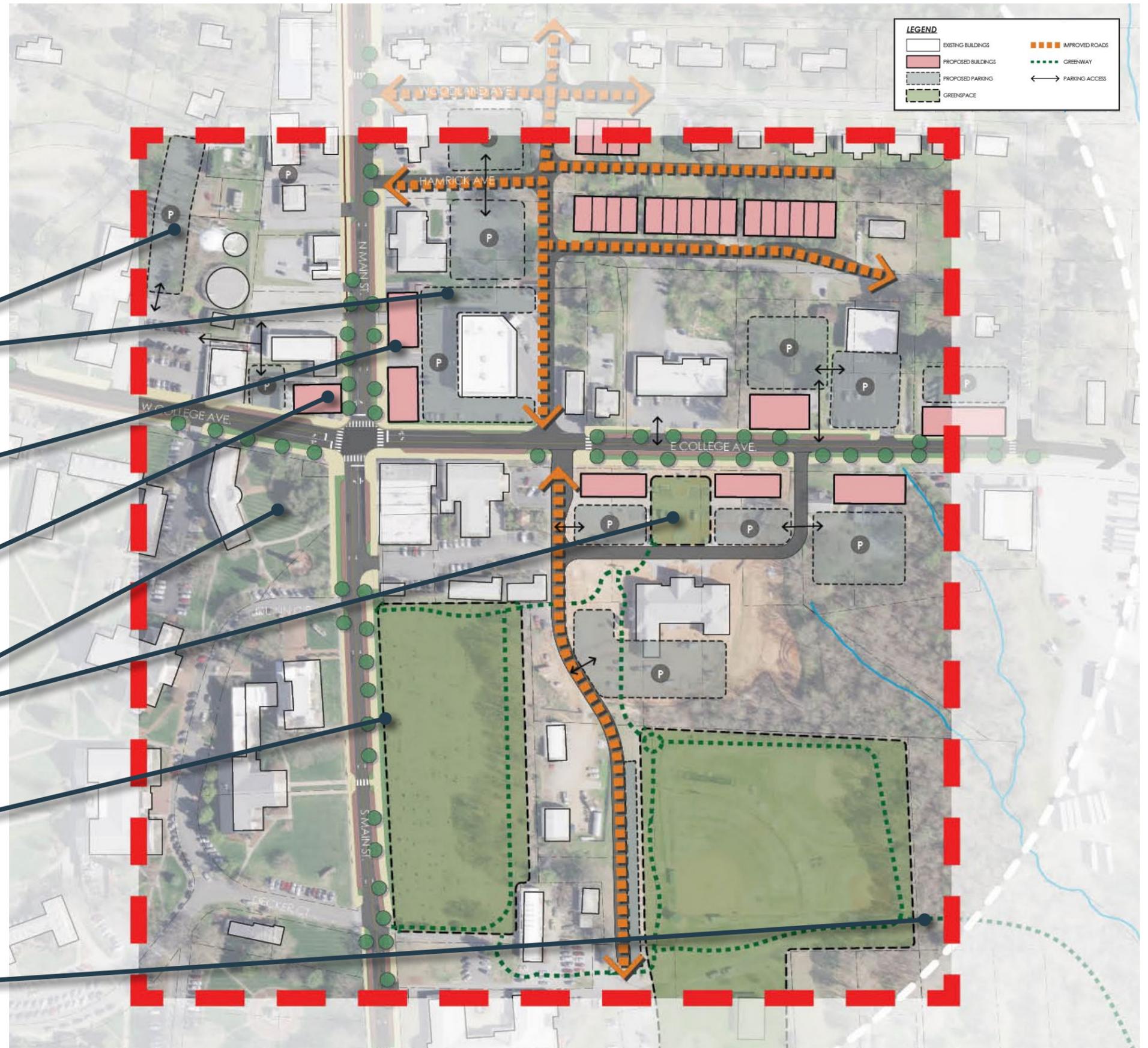
- Multi-functional + programmable spaces
- Town + Campus Events

## CEMETERY EDGE

- Improved streetscape of S. Main St
- Enhance walking paths + vegetation
- Gateway to greenway

## YMCA CONNECTOR

- Greenway connection from Town Hall to YMCA



# DESIGN AND IMPLEMENTATION STRATEGIES

BOILING SPRINGS | NC

## A) Building Architecture and Orientation -

The architectural character of buildings should augment the bedroom community charm that appeals to local community residents. When implementing new architectural and building standards, reinforcing complementary architecture and design methods that balance the existing buildings with modern housing trends is critical. Ideally, creating active storefronts are one of the main components in generating pedestrian activity with the downtown core, as well as creating a sense of place that encourages citizens to walk and bike to nearby public spaces. Several of the features and criteria below should be recognized when development occurs within the Downtown Master Plan District.

### ■ Architectural Compatible Design:

New buildings should complement existing buildings in downtown and should align the primary facade with the front property line/primary right-of-way. Parking shall be located to the rear of the building; sideyard parking shall occupy no more than twenty-five (25) percent of the primary frontage.

### ■ Building Height and Setback:

The downtown area should have a maximum building height of 35 - 40 feet or 3-stories, not including the roofline. Buildings should have a setback at a minimum of 14 to 16 feet behind the back-of-curb to allow for wider sidewalks and outdoor dining areas.

### ■ Building Materials:

Buildings should use high-quality materials that project warmth and character. Adding depth through the use of transom windows, recessed entryways, awnings or canopies, and/or other architectural fenestrations is encouraged.

## B) Public Parks, Plazas, and Open Space -

Creating passive and active open areas will strengthen community awareness. Promoting the construction and continued maintenance of public parks, playgrounds, greenway connections, and green spaces will benefit the entire community both socially and economically. Encouraging a variety of programmatic uses in public spaces to nearby citizens will garner a welcoming identity.

## C) Promote Shared Parking Opportunities -

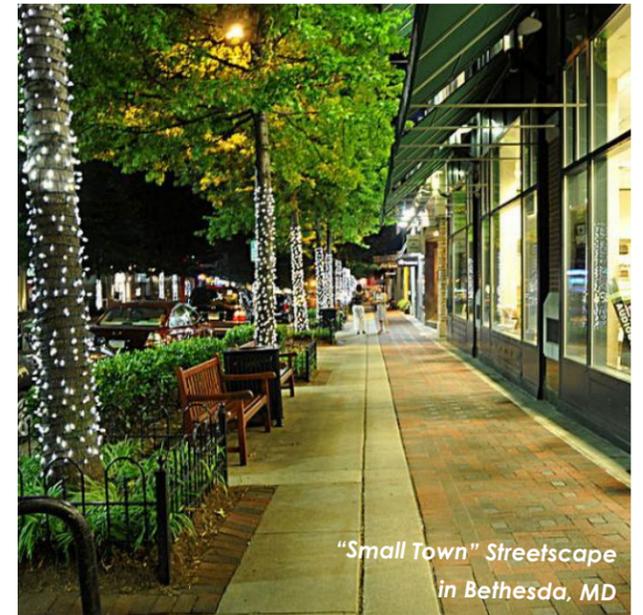
The visibility and accessibility to public and private parking facilities are crucial when shaping a successful downtown. Although parking proximity for storefront buildings is generally preferred as close to the front door as possible, losing the Town's walkability is a compromise that can no longer be afforded. Removing large parking fields immediately adjacent to Main Street & College Avenue by creating more liner retail will enhance the pedestrian experience. All new parking lots will be required to be oriented towards the rear of the building. In areas where redevelopment is preferred, connecting adjacent parking lots in an effort to reduce drive entries off the main roads will lessen traffic and increase parking availability. Providing shared parking will also encourage users to spend longer durations and visit more destinations, mutually benefiting local businesses.

## D) Road Connectivity and Infrastructure -

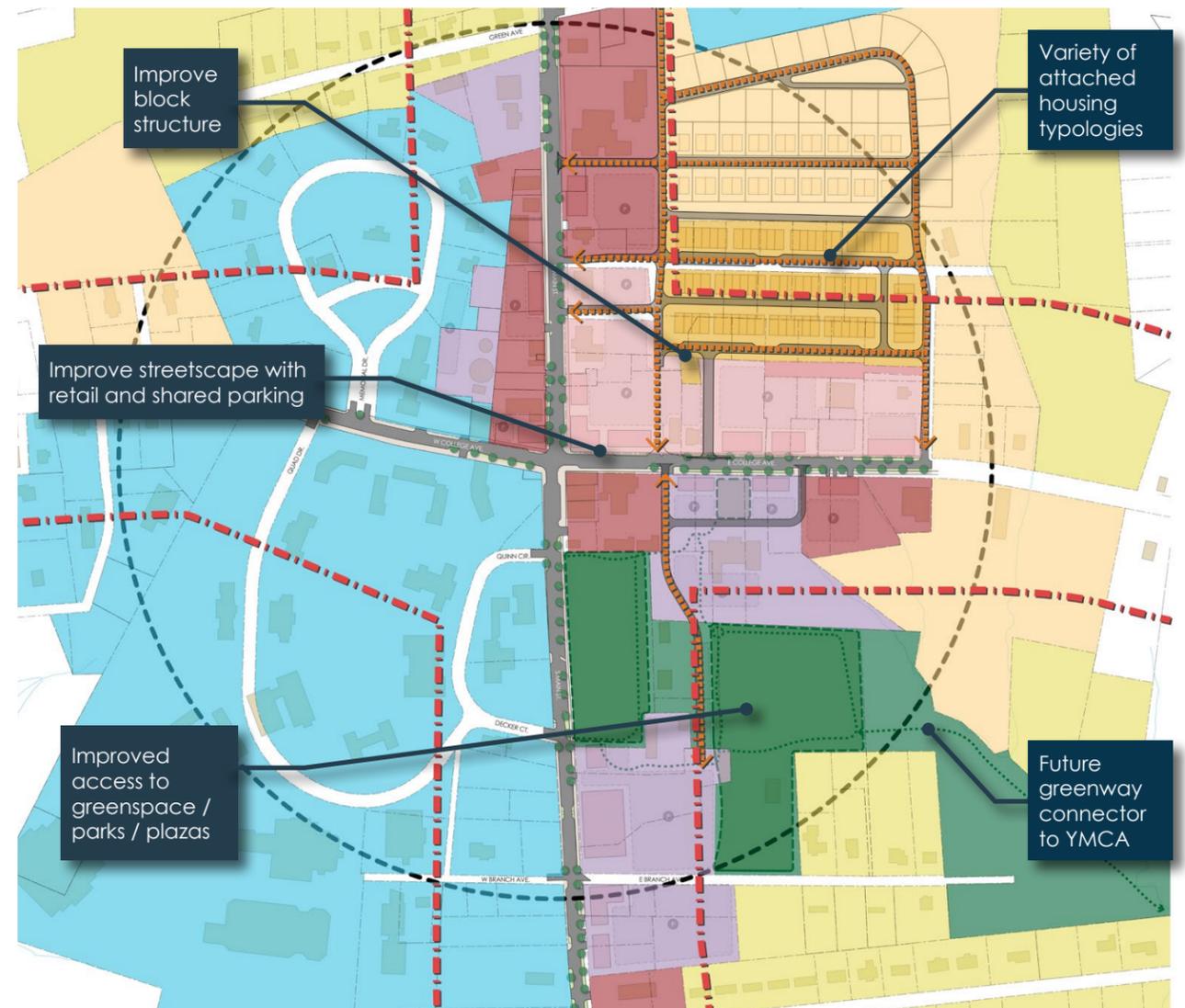
Creating walkable streets is critical in formulating a successful downtown. Introducing a smaller block structure is a simple solution, but not the only physical tool for enhancing the streetscapes. Dedicated cycling lanes, re-striping parking stalls, dieting travel lanes, enhancing cross-walk connections, and widening sidewalks are all small interventions that can have immediate community impacts. Opportunities for additional infrastructure or redevelopment exists in the surrounding community but should focus on the current market trends and housing needs in order to complement the surrounding area. Surrounding density also plays a decidedly important factor in creating successful downtown energy. Without any immediate medium- to high- density multi-family or townhomes, a void in the current housing market exists. Adding these additional housing typologies will help liven and activate the downtown area with increase density. Mixed-use buildings and developments are encouraged, especially where retail is below residential or office uses.



Madison, GA  
Town Hall Green



"Small Town" Streetscape  
in Bethesda, MD



## POLICY + MANAGEMENT & OPERATIONS STRATEGIES

BOILING SPRINGS | NC

### Policy Strategies:

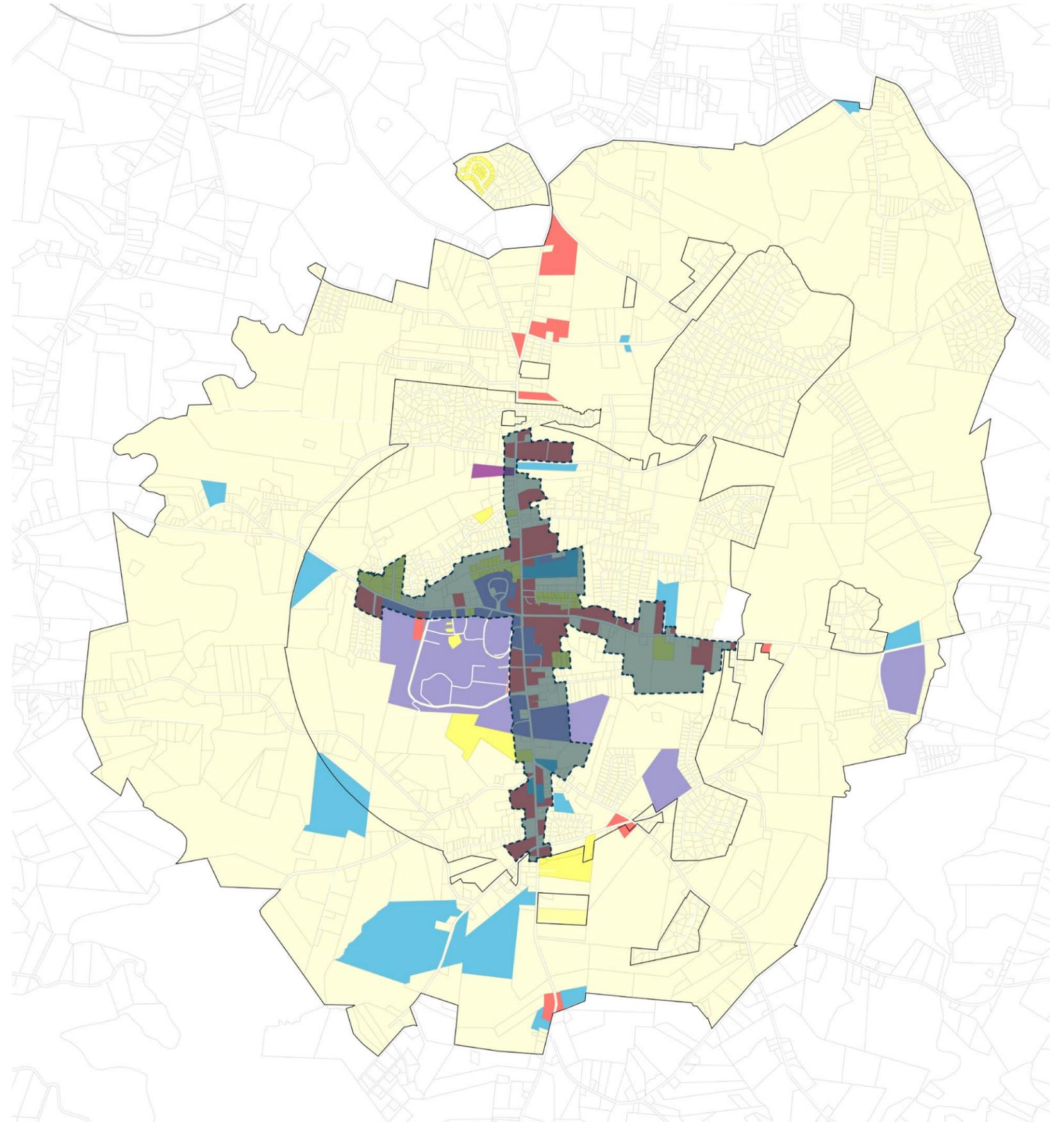
To modernize and rejuvenate the downtown Boiling Springs area, these vision and design guidelines outline the broad recommendations in key areas. Both perceptible and physical barriers are important factors to recognize while trying to create a vibrant downtown that reassures users that this is a “small town feel.” Critical considerations include:

- Bolstering partnerships with local entities including Gardner-Webb University, Broad River Greenway, North Carolina Department of Transportation, the local YMCA, and other stakeholders will assist with fostering relationships and creating event opportunities.
- Establish a Downtown District Overlay Boundary that would provide additional building standards and create build-to lines to activate the streetscape.
- Support additional retail, restaurant & storefront either through redevelopment or new development.
- Improve connectivity & access to public facilities and community amenities through interconnectivity.
- Implement Town marketing strategy, update logo, develop wayfinding/gateway signage. Utilizing the Town's Water Tower can improve branding of the community.
- Creating and enhancing gateways and public gathering spaces for collective assembly and to promote the University's relationship with the local community.
- Facilitate integration of housing types by creating additional medium- to high- density residential and adopting affordable housing initiatives.
- Update Zoning Ordinance & Land Use Plans to incorporate Master Plan strategies.

### General Strategies for Development:

In order to provide recommendations expressed in the guidelines presented, specific action items will need to be implemented by the Town of Boiling Springs in order to achieve progression towards the ideal downtown. Many of these items noted below include conditions that can be achieved through sensible design and policymaking.

- Create a downtown district specific to the 1/2 mile radius from College/Main that would provide additional standards as previously noted.
  - Promote local grants, tax credits, and programs such as affordable housing initiatives that provide additional funding through Town budget appropriations and state/federal programs.
  - Improve pedestrian and cyclist circulation by activating the streetscape and creating notable perceptible indicators for vehicular traffic flow.
- Arrange smaller public, semi-public, and private spaces with more frequent community or University programming in mind.
- Short- & long-term parking strategies that include accessibility, proximity, inner-connectivity of public/private parking facilities.



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# BOILING SPRINGS | NC CORRIDOR STUDY

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Multi-modal Corridor + Mobility Study

Public Input Summary

Recommended Master Plan + Cross Sections

Priority Projects

Crossing Recommendations

Cost Estimates



# MULTI-MODAL CORRIDOR + MOBILITY STUDY

BOILING SPRINGS | NC

The scope of this plan is to provide a comprehensive assessment of walking, bicycling, and parking along the Main Street and College Avenue corridors in the Town of Boiling Springs, including identifying pedestrian and bicyclist needs and deficiencies, recommending streetscape and amenity improvements, and prioritizing implementation strategies. **The study area for this plan focuses primarily on the downtown — a 1/4-mile radius around the intersection of these two corridors, with secondary consideration of the connections to the greater transportation network.**



EXISTING CONDITIONS IN DOWNTOWN BOILING SPRINGS.

## PROJECT BACKGROUND

The Town has recognized the opportunity to capture economic success and sustainability by expanding access for the corridor to include multiple travel modes that link to the Broad River Greenway, the YMCA, businesses, schools, and neighborhoods throughout Boiling Springs. The Corridor Study builds on past efforts and creates a complementary vision for walking and bicycling in the Town in concurrence with the ongoing Downtown Master Plan. The resulting document will be used by the Town of Boiling Springs | North Carolina Department of Transportation (NCDOT), and the Gaston-Cleveland-Lincoln MPO (GCLMPO) to prioritize, fund, and implement high-quality infrastructure, high-impact programs, and supportive policies for walking and bicycling.

This Corridor Study was made possible through funding from the Town of Boiling Springs (TOBS), the GCLMPO, NCDOT, and the Federal Highway Administration (FHWA). The Downtown Master Plan was made possible through funding from the North Carolina Department of Commerce.

## BOILING SPRINGS TODAY

Sidewalks, crossings, and bicycle lanes comprise Boiling Springs' existing pedestrian and bicycle network.

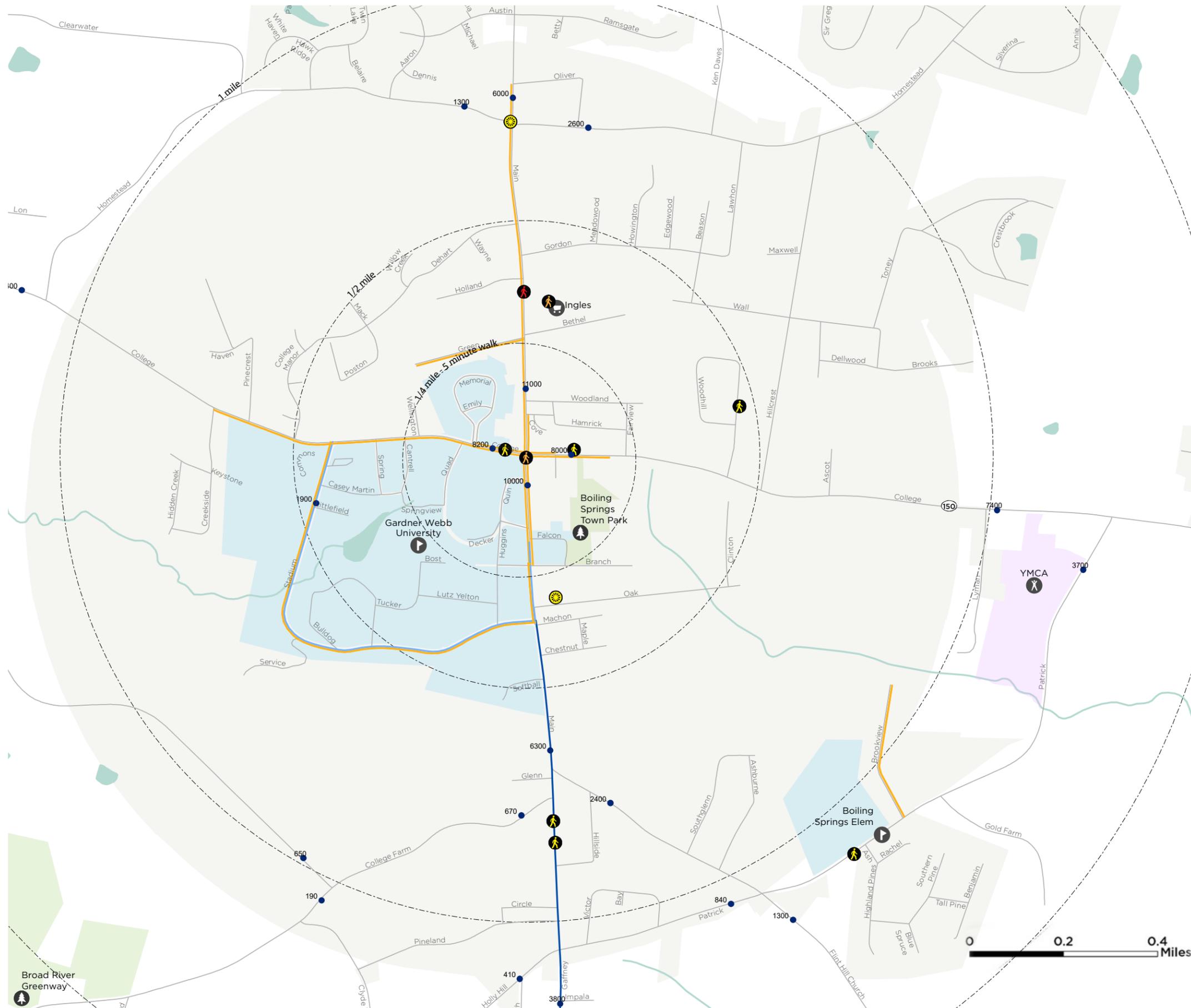
Obstacles currently facing pedestrian and bicycle travel include:

- Utility poles that create a negative impression of the streetscape and impede sidewalks
- Ada-accessibility challenges
- Perception of safety and lack of separation from cars
- Lack of streetscape amenities (lighting, landscaping, shade trees, etc.)
- Lack of well-defined routes/fragmented sidewalk network

The following 5 pages include an inventory of Boiling Springs' current transportation network, a summary of public input, and an analysis of opportunities and constraints within the study area. Discoveries included in the following pages highlight key areas where improvements are needed and inform recommendations (see page 10 for Recommendations).



(ABOVE) WORD CLOUD GENERATED DURING THE PUBLIC INPUT CHARRETTE; ATTENDEES WERE ASKED TO DESCRIBE BOILING SPRINGS TODAY, AS WELL AS THEIR VISION FOR THE TOWN IN A SINGLE WORD OR PHRASE (SEE PAGE 11 FOR VISION WORD CLOUD).



## EXISTING CONDITIONS

### Pedestrian Facilities

Existing Sidewalk

### Bike Facilities

Existing Bike Lane - One Side

Existing Bike Lane - Both Sides

### Pedestrian Crashes 2007-2016

- Disabling Injury
- Evident Injury
- Possible Injury
- Killed

### Bike Crashes 2007-2016

- No Injury

### Other

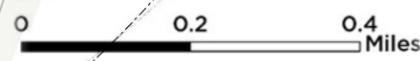
- Traffic Counts 2016-2017 (AADT)

### Destinations

- Park
- School
- Grocery Store
- YMCA

### General

- Roads
- Boiling Springs Town Limits
- Lakes and Ponds
- Streams



## PUBLIC INPUT SUMMARY

As part of the data collection process and design development, the project team solicited input from Boiling Springs residents, community leaders, and project stakeholders. Feedback was collected in a number of ways to be the most inclusive and representative of the community make-up. This includes:

- **3 steering committee meetings** - The steering committee was comprised of staff from NCDOT and the GLCMPO, TOBS staff, the YMCA, Gardner-Webb University, and local business owners and community leaders. The project team met with the steering committee in November 2018, May 2019, and September 2019 to get input and feedback on project priorities.
- **5 stakeholder focus group meetings** - Stakeholders included NCDOT, the YMCA, the Broad River Greenway, Gardner-Webb University, and local business owners.
- **2-day public input session/charrette** with a pin-up presentation (open to the public) - This session was held over the course of 2 days, June 24-25 2019, to get initial feedback and reactions to initial proposed concepts. The public input session on June 24 included an overview presentation about the project and break out sessions around maps where community members shared input on local issues, opportunities, and priorities. Community members also provided feedback on key elements of land use and streetscape elements that they'd like to see via precedent boards. On the evening of June 25, the consultant team presented draft concept recommendations for community feedback.
- Additional public input was provided when the draft master plan was posted to the Town Website and when the draft plans were presented to the Town Board for consideration.

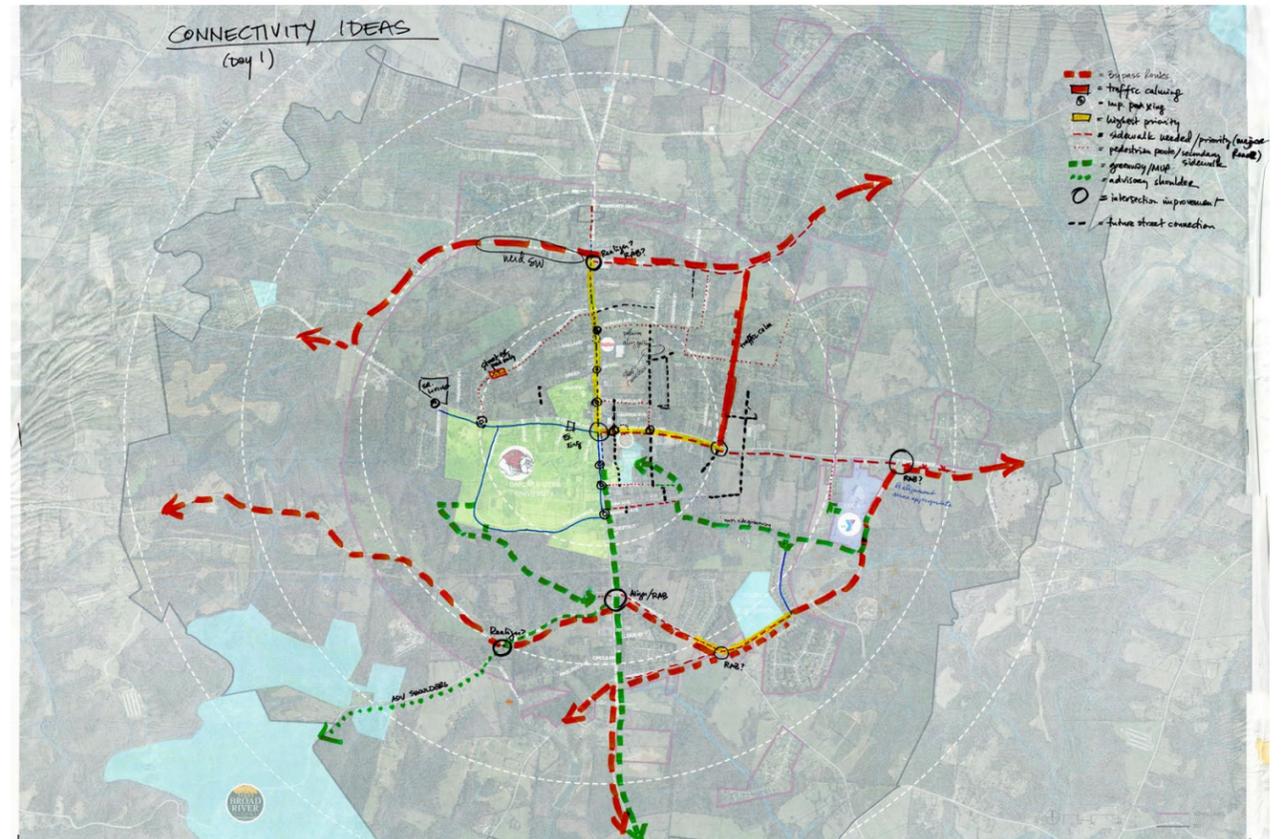


LOCAL RESIDENTS AND STAKEHOLDERS AT THE JUNE 2019 DESIGN CHARRETTE AND PIN-UP PRESENTATION.

## PRECEDENT BOARDS + CONNECTIVITY IDEAS

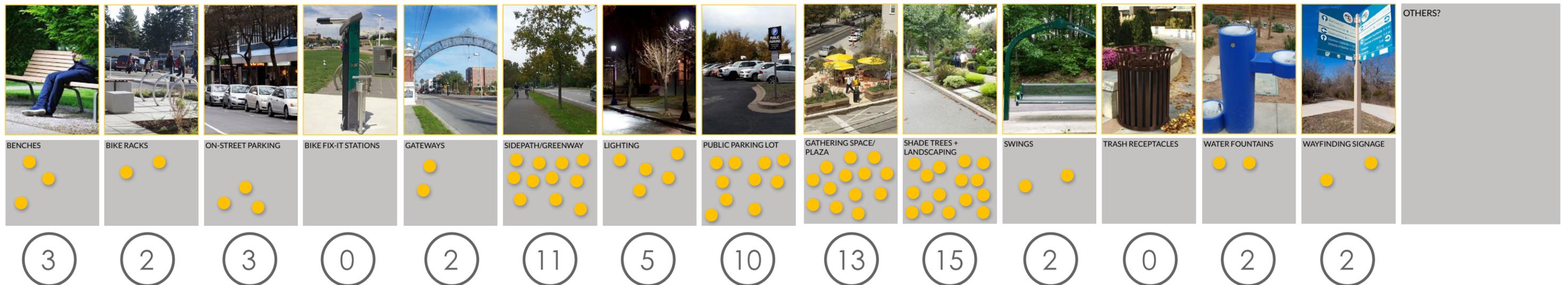
The workshop was the Discovery phase of the project. Attendees were asked to share what they perceived as opportunities and constraints in the Town (map on the following spread, pages 8-9). They were also asked to vote on which streetscape and greenway amenities they'd like to see in Boiling Springs (responses below). These prompts allow the project team to establish a shared vision for Downtown Boiling Springs and guide the design efforts during the workshop.

The team conducted more fieldwork and traffic observation during Day 2 of the workshop and developed initial recommendations. At the end of the day, a pin-up session open to the public allowing the recommendations to be seen by local residents and community leaders. After receiving feedback at the pin-up session, the project team refined their recommendations.

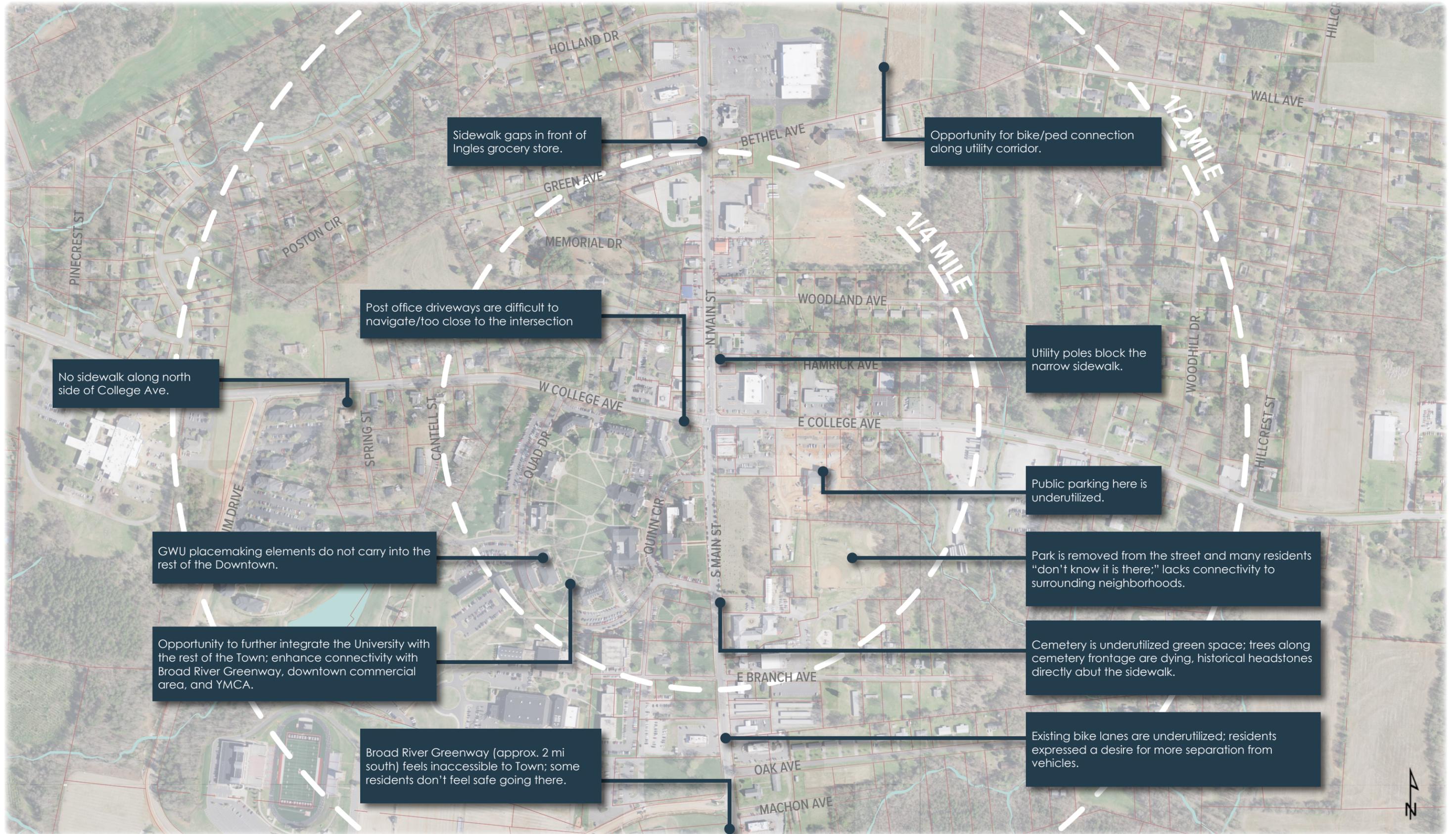


(ABOVE) DRAFT NETWORK RECOMMENDATIONS DEVELOPED DURING THE CHARRETTE.

### Public Input Priorities: Streetscape and Greenway Amenities



# Opportunities and Constraints Map





## COMMUNITY STRENGTHS

- Friendly
- Low crime rate
- Good Town/University relationship; lends an atmosphere of culture and a great asset for events
- Town Park
- Lots of walkers, runners, and cyclists

## TOP OPPORTUNITIES

- **Downtown Hub** - increase number/variety of restaurants, entertainment, and retail to draw Greenway users in to spend money and support tourism, and to encourage residents to shop locally rather than driving to surrounding communities
- Increase the visibility of and **connectivity to existing town assets and neighborhoods with 'local' greenways and bike-ped connectors**, improve existing facilities and crossings to encourage walking and biking
- **Streetscape along Main Street and College Avenue** - an opportunity to underground utilities to improve ADA-accessibility and the overall look of the streetscape, consolidate driveways and reconfigure travel lanes to make room for more on-street parking and wider pedestrian space
- **Placemaking and branding** - expand upon GWU branding and wayfinding into the rest of the Downtown to enhance the overall sense of place, consistent streetscape amenities (lighting, seating, landscaping, public art, etc.)
- **Housing options** - Cater to a wider population of potential residents (students and retirees) with new housing developments that are denser, affordable, and on smaller lots closer to Town
- **Consolidation of civic uses** - relocate post office (possibly to Town Hall or vacant building) to allow the current parcel to redevelop, explore the possibility of a municipal library in a more centralized location
- **Policy updates** - create a set of development and design standards (form-based code) that will guide future redevelopment, code enforcement

## CORRIDOR STUDY RECOMMENDATIONS

The Main Street and College Avenue corridor recommendations are intended to be supportive of the long-term vision for a balanced mobility system in Downtown Boiling Springs that sets walking and biking on par with automobile use. The recommended projects can be implemented in phases or together to form a more cohesive complete street network. The corridor concept was rooted in the shared vision and goals developed by Boiling Springs residents and stakeholders. The vision statement and guiding principles can be found on the facing page.

This section details recommendations for the Boiling Springs. The intent of these recommendations is to present a framework for improving access for active travel modes, with an emphasis on pedestrians, ensuring accessibility and mobility for pedestrians and bicyclists of all ages and abilities. Improvements focus on making walking and biking safer and more enjoyable experience for residents and visitors.

To achieve such a vision, the recommendations are organized as follows:

- Project vision + guiding principles
- Town-wide Network Map
- Downtown Conceptual Plan
- Recommended Cross Sections
- Downtown Parking Supply
- Priority Projects
- Wayfinding Concept + Streetscape Amenities
- Crossing Improvements

Recommendations within this plan should reference the NCDOT Complete Streets Planning and Design Guide for design guidelines. See <https://connect.ncdot.gov/projects/BikePed/Pages/Complete-Streets.aspx> for more details.

## VISION STATEMENT

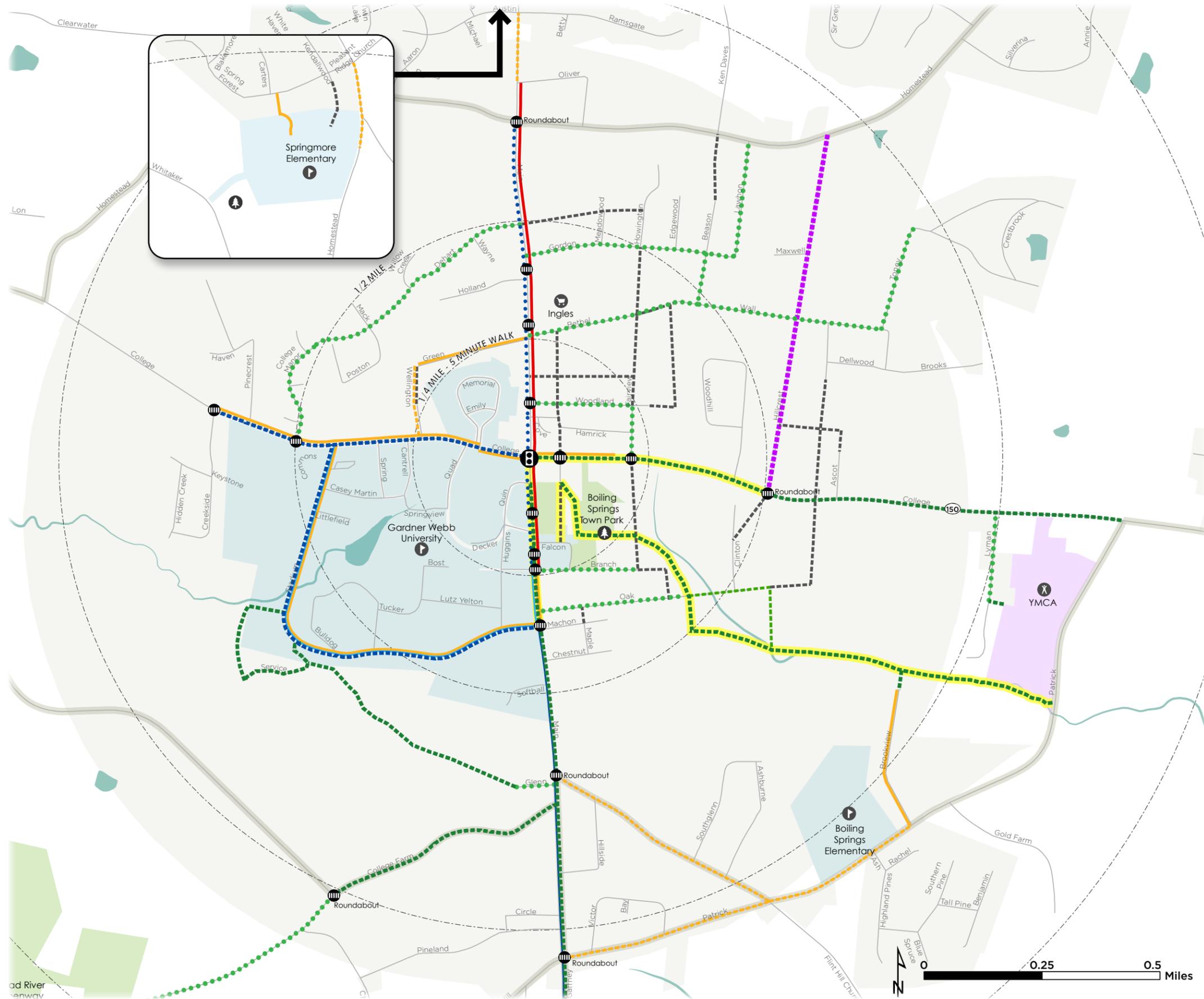
Main Street and College Avenue will serve residents and tourists of Boiling Springs as **vibrant, active transportation corridors** that will connect the Town to the Broad River Greenway, spur **economic growth**, provide **multi-modal connectivity**, and strengthen the **community's identity** as a thriving college town that is a great place to live, work, and play.



(ABOVE) WORD CLOUD GENERATED DURING THE PUBLIC INPUT CHARRETTE; ATTENDEES WERE ASKED TO DESCRIBE THEIR VISION FOR BOILING SPRINGS TODAY IN A SINGLE WORD OR PHRASE (SEE PAGE 4 FOR BOILING SPRINGS TODAY WORD CLOUD).

## GUIDING PRINCIPLES

- Provide multi-modal, Complete Street corridors that give residents and visitors **transportation choices**
- Make a seamless shared-use **path connection from Downtown to the Broad River Greenway** that is safe, accessible, and convenient for all ages and abilities
- Carry design elements from Gardner-Webb University throughout the Downtown streetscape to enhance community identity
- Accommodate vehicle traffic and parking needs in the 1/4-mile radius to support future redevelopment in Downtown
- Ensure safe intersection and driveway crossings, especially for people walking and biking
- Create inviting, vibrant corridors that attract businesses, residents, economic development, and tourism
- Create a stronger sense of place that is currently lacking outside of the University campus
- Ensure compliance with national and NCDOT standards, design guidelines, and best practices



## NETWORK RECOMMENDATIONS

### Pedestrian Facilities

- Existing Sidewalk
- Proposed Sidewalk (priority)
- Existing Sidewalk - Needs Improvements
- Proposed Multi-Use Path/Greenway
- Proposed Advisory Shoulder/Shared Street
- Proposed Bicycle/Pedestrian Connection (off-street)

### Bike Facilities

- Existing Bike Lane - Both Sides
- Existing Bike Lane - One Side
- Proposed Bike Lane
- Proposed Shared Lane Markings

### Intersection Improvements

- B Signalized
- X Unsignalized

### Other Network Improvements

- Proposed Traffic Calming
- Proposed Street Connection (built through redevelopment)
- Bypass Route
- High Priority

### Destinations

- Park
- Recreation Center
- School
- Grocery Store

### General

- Streets
- Streams
- Lakes and Ponds
- Boiling Springs Limits



## CONCEPTUAL PLAN

In addition to the network map, the Downtown Conceptual Plan (see left) was developed to provide a visual overview of the many interconnected projects in Boiling Springs' downtown core, including reconfiguring lanes, retrofitting on-street parking along Main and College, expanding the sidewalk and bicycle network, and crossing improvements to offer more midblock options for pedestrians. The details of these projects, and others, are outlined in the following pages.

-  Potential off-street parking opportunity
-  Existing and potential green space
-  On-street parking (with enhanced pavement)

*Please note: This is not a design plan; precise locations and elements should be designed in accordance with engineering standards and NCDOT review.*

*Please see the Master Plan for details on land use and redevelopment shown in this plan.*

## RECOMMENDED CROSS SECTIONS

The following 8 pages include recommendations for the corridors within Boiling Springs. These include Downtown long-term cross sections and how they transition as the corridor moves outwards to a more rural context.

The proposed cross sections are broken into the following 9 segments:

1. N Main Street (College Avenue to Green/Bethel Avenue) pg 15
2. N Main Street (Green/Bethel Avenue to Homestead Avenue) pg 16
3. S Main Street (College Avenue to Branch Avenue) pg 17
4. S Main Street (Branch Avenue to Broad River Greenway) pg 18
5. W College Avenue (Main Street to Creekside Drive) pg 19
6. E College Avenue (Main Street to Fairview Street) pg 20
7. E College Avenue (Fairview Street to Patrick Avenue) pg 21
8. Typical Neighborhood Street pg 21

Aspirational cross sections represent the long-term vision for the corridor and are considered to be recommendations for the typical conditions when a future NCDOT project occurs or when build-out has occurred through the development and/or redevelopment. Interim recommendations are near-term complete street upgrades that can be accomplished without needing to move the existing curb and could eventually transition to a long-term section. **All cross sections should include improvements to streetscape amenities, pedestrian-scale street lights, wayfinding signage, and signalized and un-signalized crossings.**

# NORTH MAIN STREET

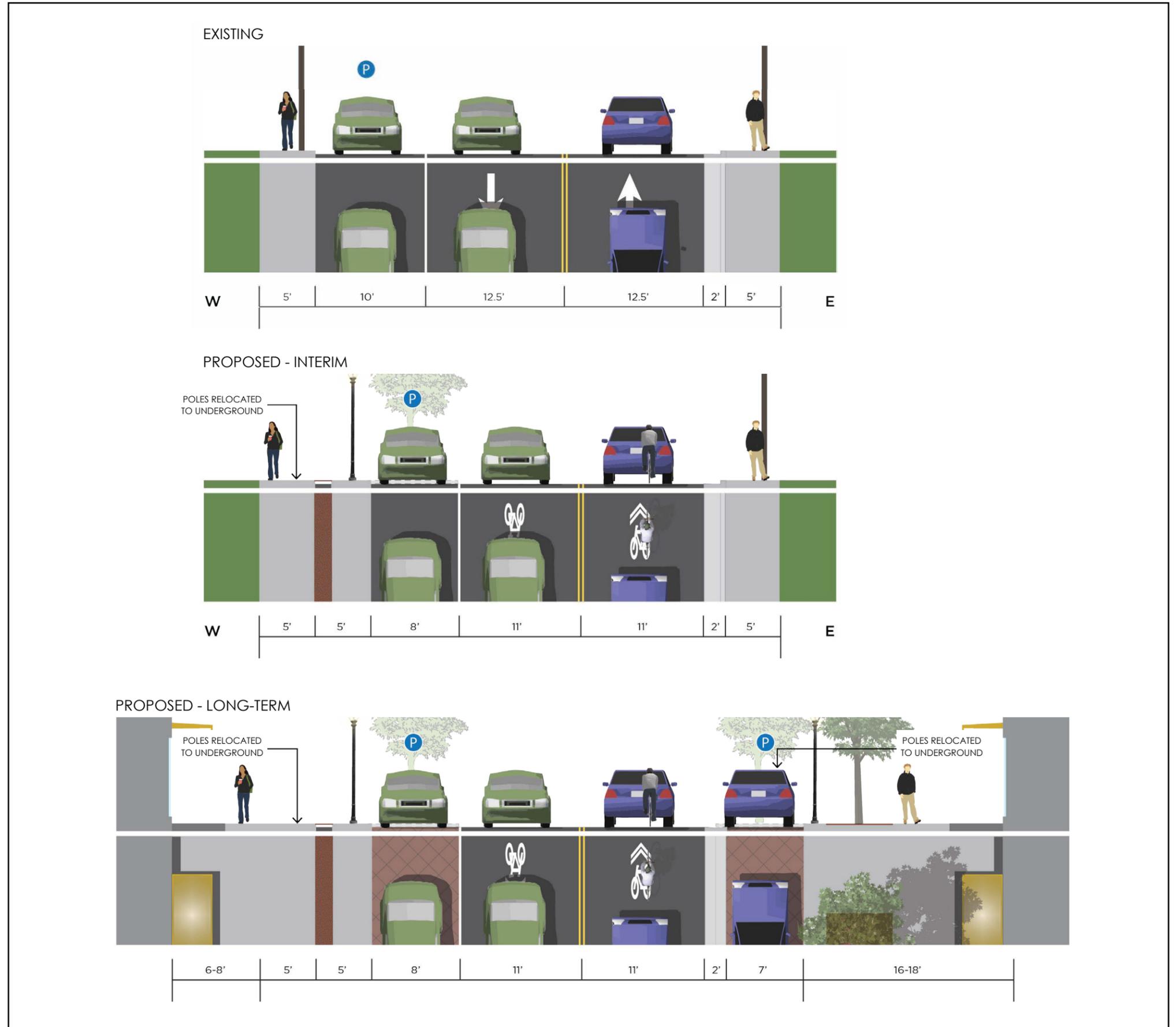
COLLEGE AVENUE TO GREEN/BETHEL AVENUE

This segment is characterized by two wide travel lanes and a wide parking lane on the west, protected in some areas by curb extensions and spherical concrete bollards (recently relocated to the Town Park). Existing pedestrian facilities have ADA-compliance issues - sidewalks are narrow and impeded by light poles. A variety of businesses front North Main Street, although setback from the street varies and many buildings are separated from the street by parking lots. Many lots have more than one curb cut, creating unnecessary driveways and segmenting the pedestrian environment.

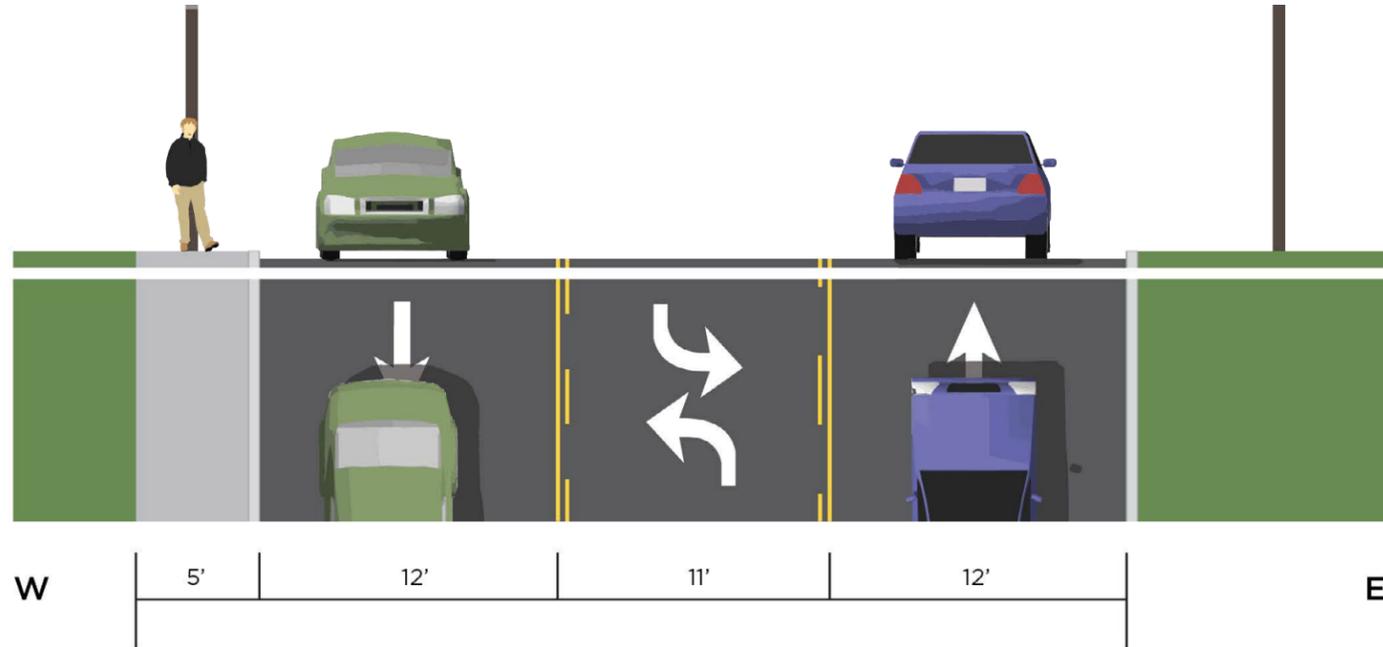
**“Floating” curb extensions connected to the sidewalk with flush linear drainage grates eliminate the need for curb reconstruction in both the interim and aspirational conditions.** The curb extensions create space for street trees and pedestrian lighting, while also narrowing the street to encourage traffic calming. As redevelopment occurs, a parking lane on the east side of Main Street and a 16 to 18-foot frontage setback supports a more traditional “downtown” environment, and creates space for sidewalk retail, outdoor dining, landscaping, and street furniture.



(ABOVE) STREET VIEW ALONG N MAIN STREET. BOLLARDS HAVE SINCE BEEN RELOCATED.



EXISTING



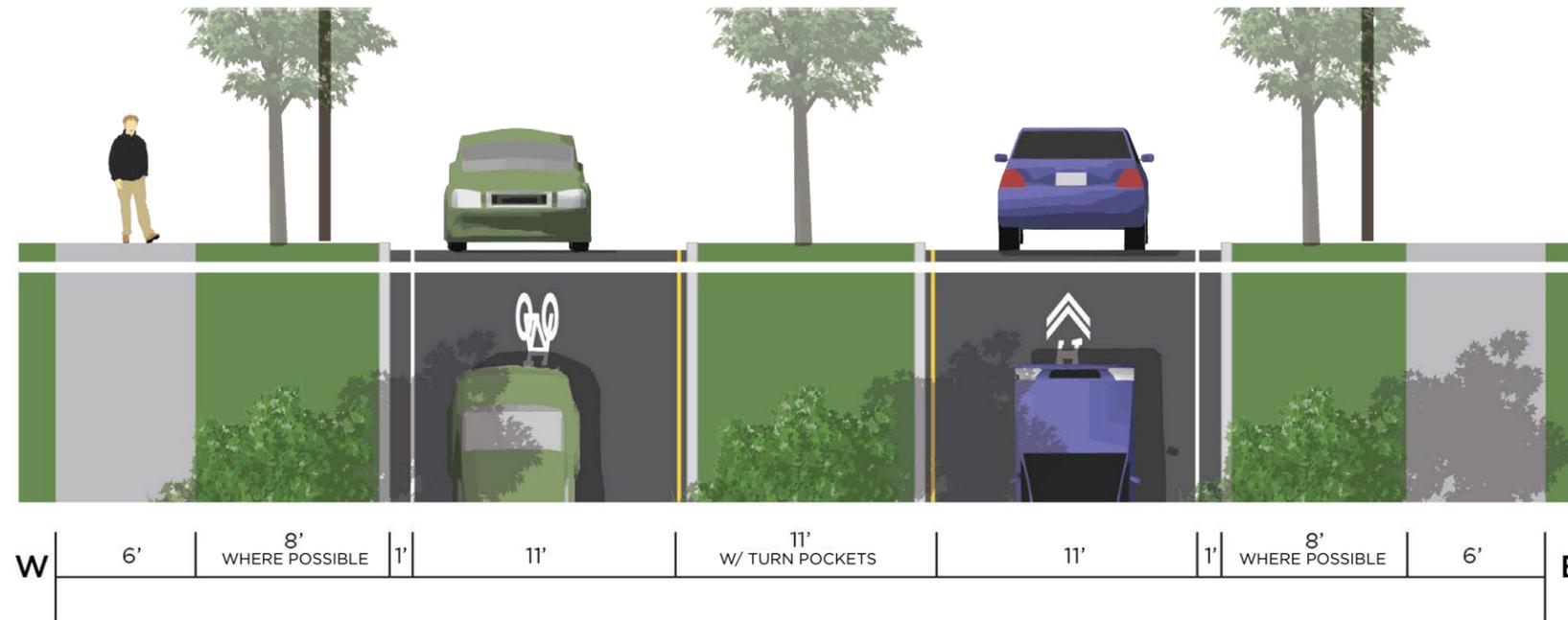
## NORTH MAIN STREET

GREEN/BETHEL AVENUE TO HOMESTEAD AVENUE

As North Main Street transitions to a residential context, it becomes a two lane road with a two-way center turn lane. Light poles sit directly in the middle of existing sidewalks.

Recommendations for this segment do not require any curb reconstruction, but rather painted edgelines on either side of the travel lane to visually narrow the road and encourage slower speeds. Spot medians with street trees are recommended along North Main Street with left turn lanes at strategic locations (such as neighborhood entrances or major roads). There is leftover right-of-way outside of the edge of pavement to accommodate wider planting strips and sidewalks setback from the road whenever possible. **Planting strips allow existing utility poles to remain without the need for relocation and create space for more street trees** that will shade pedestrians and contribute to traffic calming.

PROPOSED



(ABOVE) STREET VIEW ALONG N MAIN STREET

# SOUTH MAIN STREET

COLLEGE AVENUE TO BRANCH AVENUE

Travel lanes in this segment are oversized, leaving minimal space for pedestrian facilities and streetscape amenities. The existing sidewalk on the cemetery frontage is narrow and closely borders historic headstones and decaying trees. Currently, there are no bike facilities on this segment of South Main Street; on-street bike lanes south of Branch Avenue transition into on-street parking as they approach College Avenue.

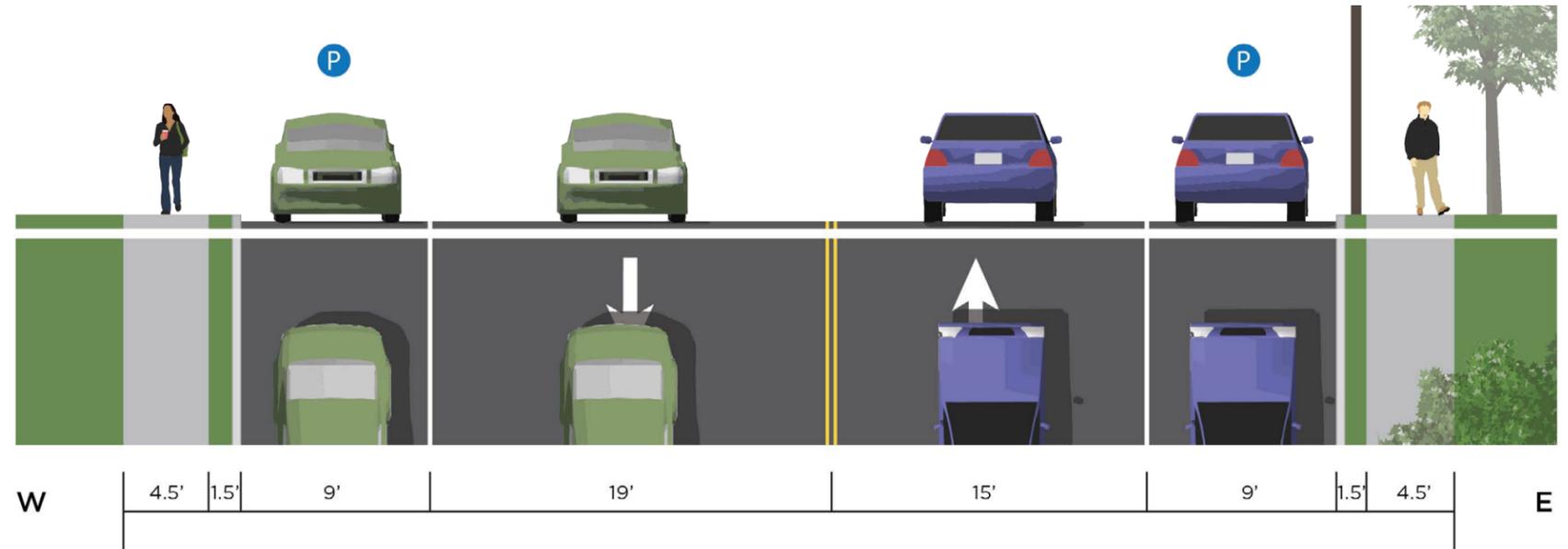
Recommendations for this segment include reconstruction of the east curb, gaining 7-feet of green space in between cemetery frontage and the proposed 12-foot multi-use path. The curb extensions and wider planting strip allow for street trees and landscaping, contributing to a more vibrant pedestrian environment and traditional "downtown" feel. **If an interim solution is desired, the road reconfiguration could be accomplished by designating existing pavement as pedestrian space with paint and bollards (or another low-cost buffer material).**

For more details, please refer to the Priority Projects later in this chapter, page 26.

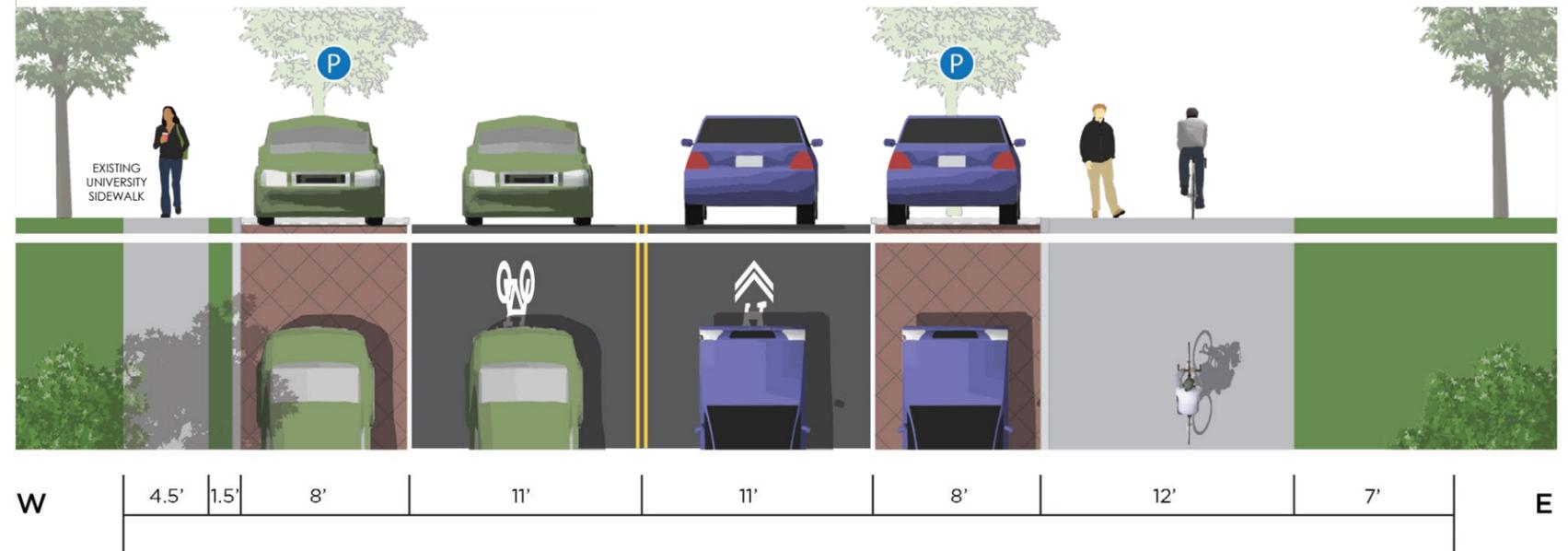


(ABOVE) STREET VIEW ALONG S MAIN STREET

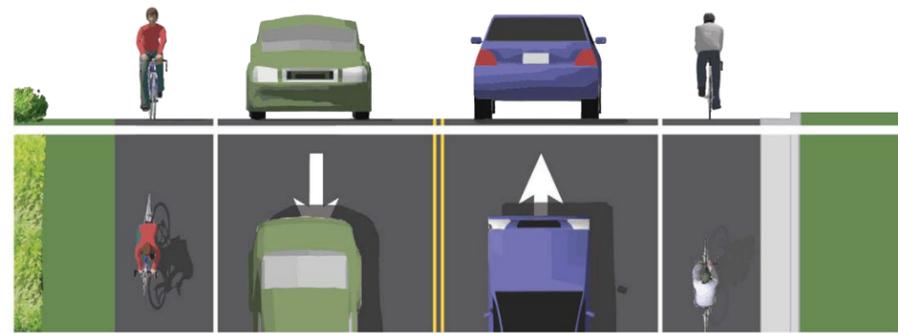
EXISTING



PROPOSED



EXISTING



PROPOSED - EXISTING CURB + GUTTER



PROPOSED - NO CURB + GUTTER



## SOUTH MAIN STREET

BRANCH AVENUE TO BROAD RIVER GREENWAY

South Main Street transitions to a more rural context as it moves south towards the Broad River Greenway, with single-family homes and some isolated businesses. Standard on-street bike lanes run alongside the two travel lanes reaching the Greenway trailhead, approximately 3 miles south. There is no sidewalk along South Main Street and pedestrians often walk in the bike lane or in the grass to reach their destinations, indicating a true need and demand for proper facilities.

Recommendations include a lane width reduction, holding the existing edge of pavement to the west. A 10-foot multi-use path is proposed on the east side of the street, improving safe and comfortable access to the businesses and neighborhoods along the corridor and strengthening the community's connection to the Broad River Greenway, an important recreational amenity for residents and visitors. The sidepath makes use of the leftover right-of-way by maintaining separation from the road, leaving room for shade trees, and green infrastructure/swales when appropriate. **Curb and gutter is only recommended in the sections where the infrastructure already exists to minimize costs, and should only be implemented within the 1/2-mile radius of Downtown.**



(ABOVE) STREET VIEW ALONG S MAIN STREET

# WEST COLLEGE AVENUE

MAIN STREET TO CREEKSID DRIVE

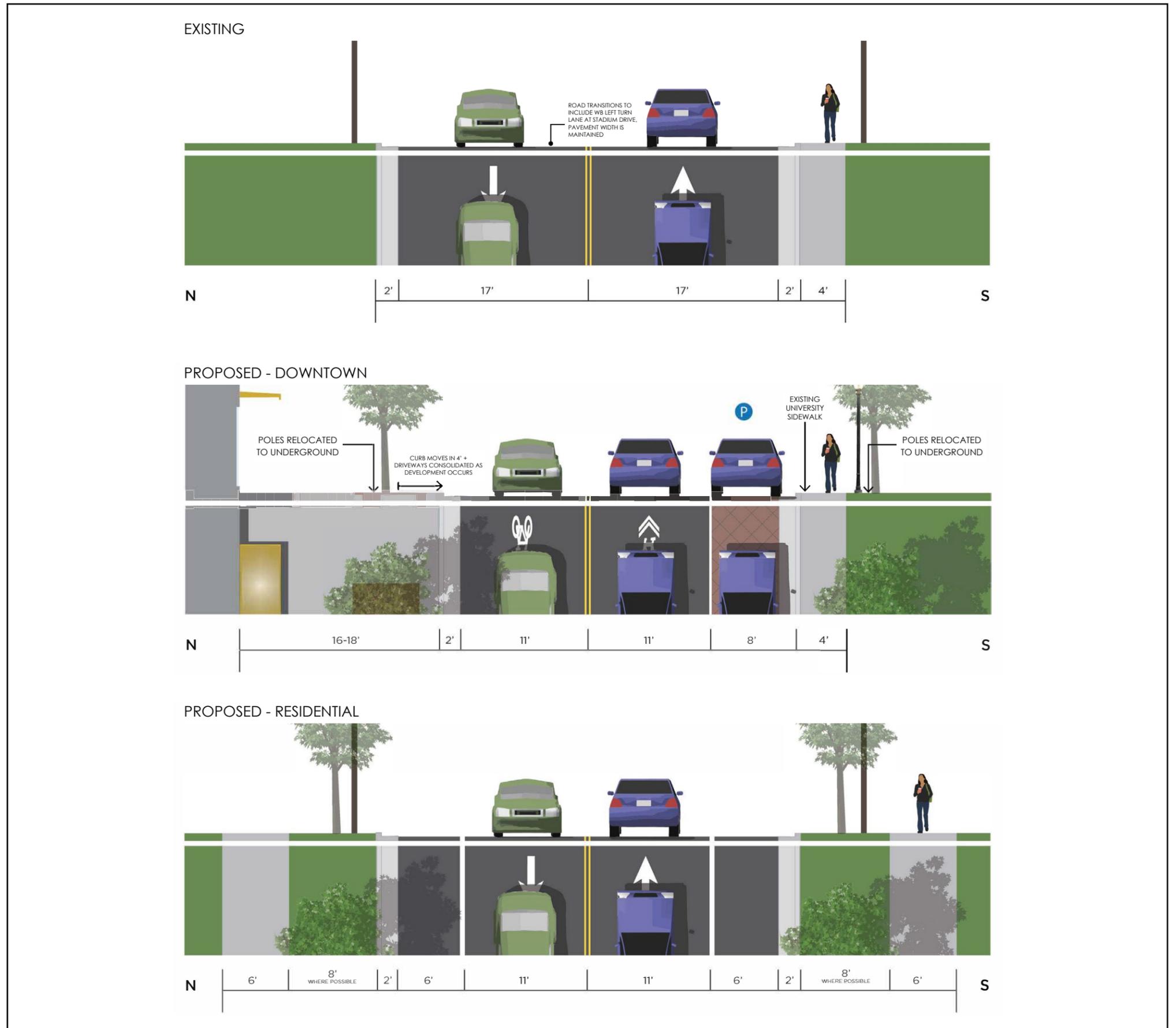
The existing West College Avenue cross section is relatively consistent from the downtown area into a residential context, with two wide travel lanes and limited pedestrian and bicycle facilities.

For the immediate downtown area (approximately 1 block), a condition similar to the downtown North Main Street segment keeps the same rhythm and supports a connected street grid commonly found in downtown areas. The south side of the road is adjacent to the northern edge of Gardner-Webb University and is not expected to redevelop; **maintaining the curb and adding a parking lane serves the immediate needs of residents and visitors while the north side has more flexibility to develop over time.** As redevelopment occurs, consolidation of driveways and a 16 to 18-foot frontage setback supports new businesses and creates space for sidewalk retail, outdoor dining, landscaping, and street furniture.

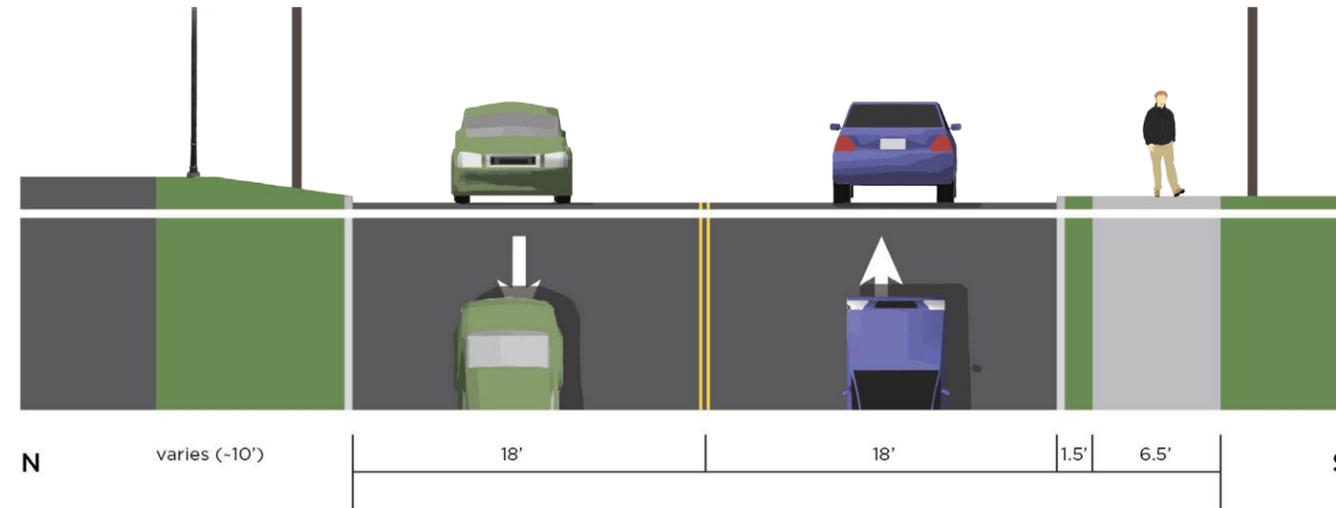
**As West College Avenue continues west past Memorial Drive, the curb bumps back out to its original location to minimize cost and construction.** 6-foot bike lanes with extra-wide white lines narrow the travel lanes and improve bicycle connectivity west. Sidewalks are shown with a planting strip whenever feasible, to accommodate existing utility poles and street trees and create a comfortable walking condition for residents.



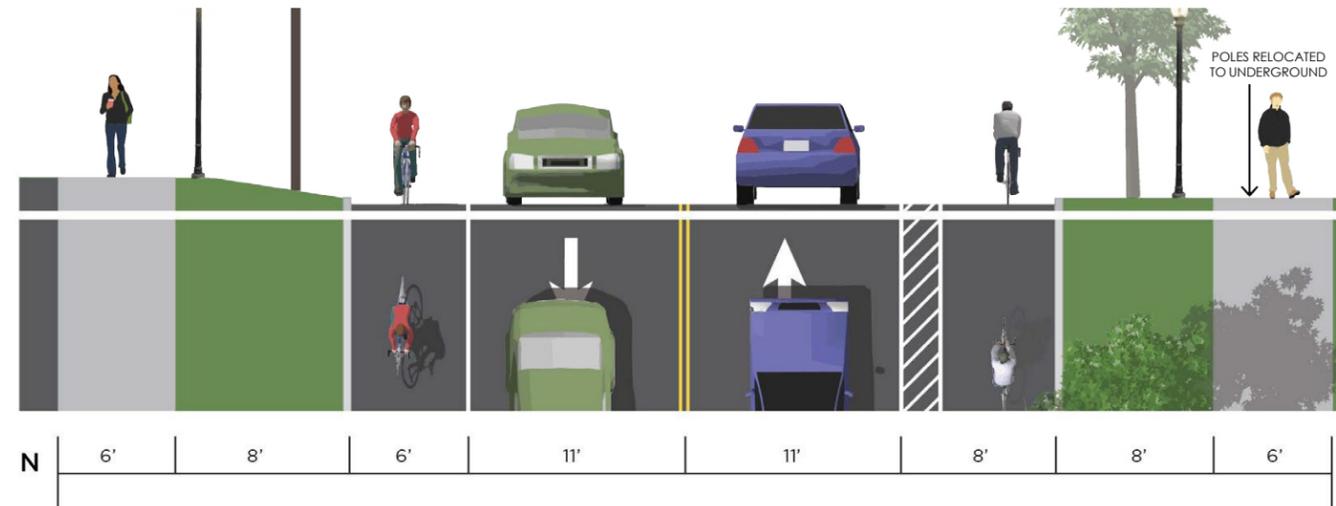
(ABOVE) STREET VIEW ALONG W COLLEGE AVENUE



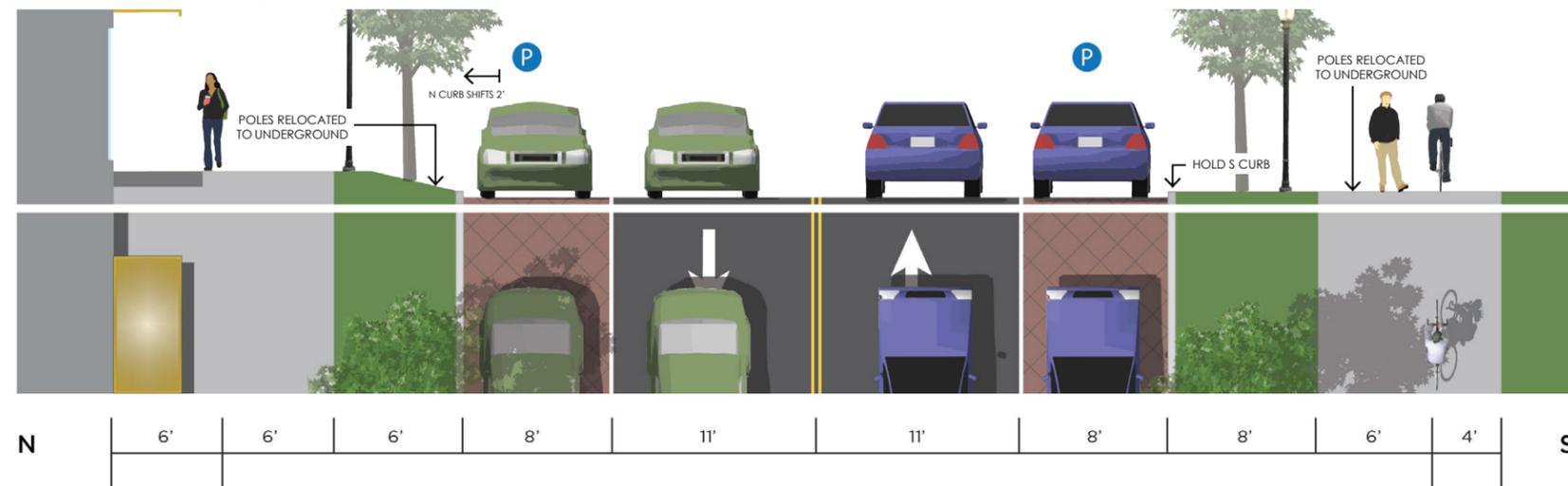
EXISTING



PROPOSED - INTERIM



PROPOSED - LONG-TERM



## EAST COLLEGE AVENUE

MAIN STREET TO FAIRVIEW STREET

East College Avenue has two wide travel lanes, surrounded by the Town Hall on the south and some businesses and residences on the north. There is some existing sidewalk on this segment; on the south and about half of the north side.

Interim recommendations make use of the extra lane width and include 6-foot bike lanes, buffered whenever possible. The southern sidewalk is pushed back to accommodate a planting strip with pedestrian-scale lighting and street trees. With redevelopment and a future widening on the north side of the road, the sidewalk could be widened to a 10-foot sidepath that accommodates bikes and pedestrians and the bike lanes transitioned into on-street parking. The sidepath could continue east to reach the YMCA on Patrick Avenue, and improve connectivity to the park behind Town Hall. There is also an opportunity to program the green space between Town Hall and the sidepath.

For more details, please refer to the Priority Projects later in this chapter, page 28.



(ABOVE) STREET VIEW ALONG E COLLEGE AVENUE

## EAST COLLEGE AVENUE

FAIRVIEW STREET TO PATRICK AVENUE

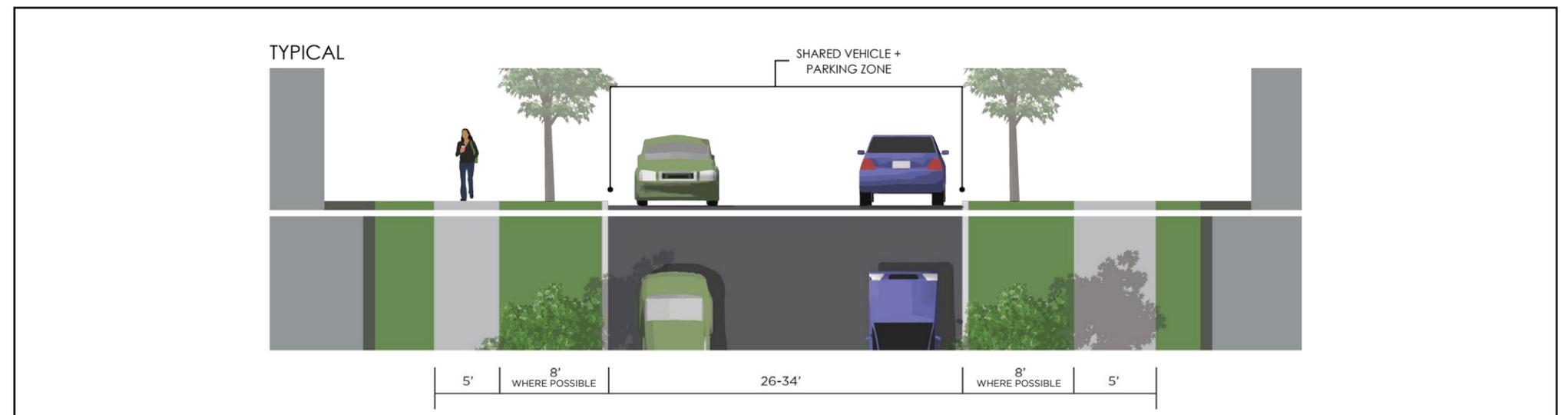
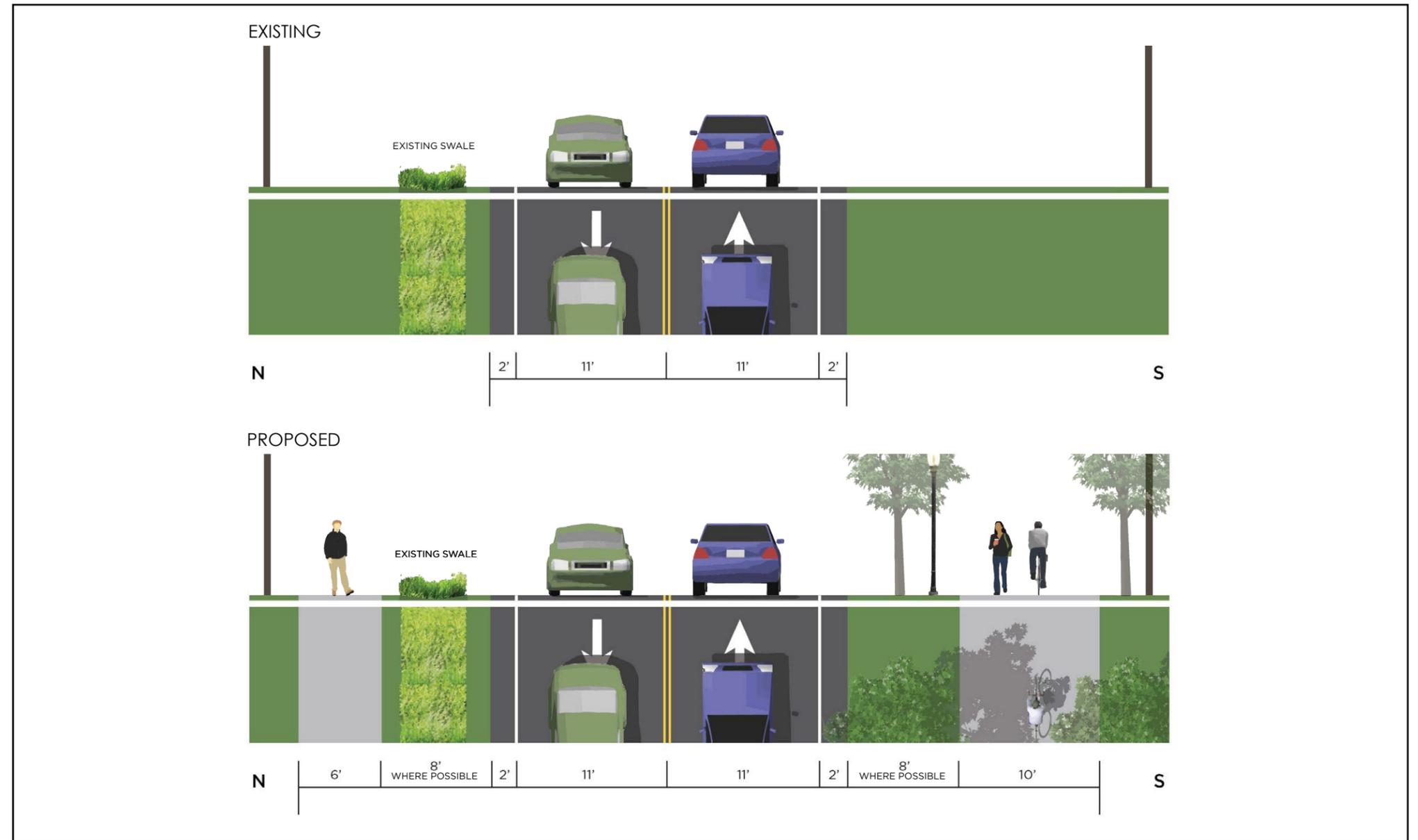
Recommendations for East College Avenue include maintaining the existing edge of pavement and travel lanes. Pedestrian and bicycle facilities are currently missing from this segment, except for a small section in front of a new development on the north side. A 6-foot sidewalk and continuation of the 10-foot sidepath on the south side are recommended to improve connectivity to downtown from the east side of the Town. Street trees, pedestrian-scale lighting, and wayfinding should be installed in the planting strip whenever appropriate on either side of the road.



(ABOVE) STREET VIEW ALONG W COLLEGE AVENUE

## TYPICAL NEIGHBORHOOD STREET

As described in the NCDOT Complete Streets Planning and Design Guidelines, a typical neighborhood street section is designed for low-speed, low-volume residential areas. A minimum 26-foot pavement width accommodates mixed traffic with cars, bicycles, and on-street parking on one side. This section may be appropriate for existing and future single- and multi-family subdivisions in Boiling Springs.



## DOWNTOWN PARKING SUPPLY

In addition to off-street parking supply additions expected to come with future development (described in the Downtown Master Plan), there are numerous opportunities to add on-street spaces in the near term. The graphics below and on the facing page depict the on-street parking opportunities that the Town should explore when streets are repaved, reconfigured, or privately redeveloped.

Currently, there are 70 on-street parking spaces in Downtown Boiling Springs. With the implementation of streetscape projects that are proposed in this plan, approximately 225 spaces will be made available. This is in addition to publicly available parking spaces in Town-owned lots at Town Hall, the park, and next to the cemetery. The Town could also work with downtown businesses and GWU on shared parking agreements for events and other parking needs.

Main Street Corridor - Existing Parking



College Avenue Corridor - Existing Parking

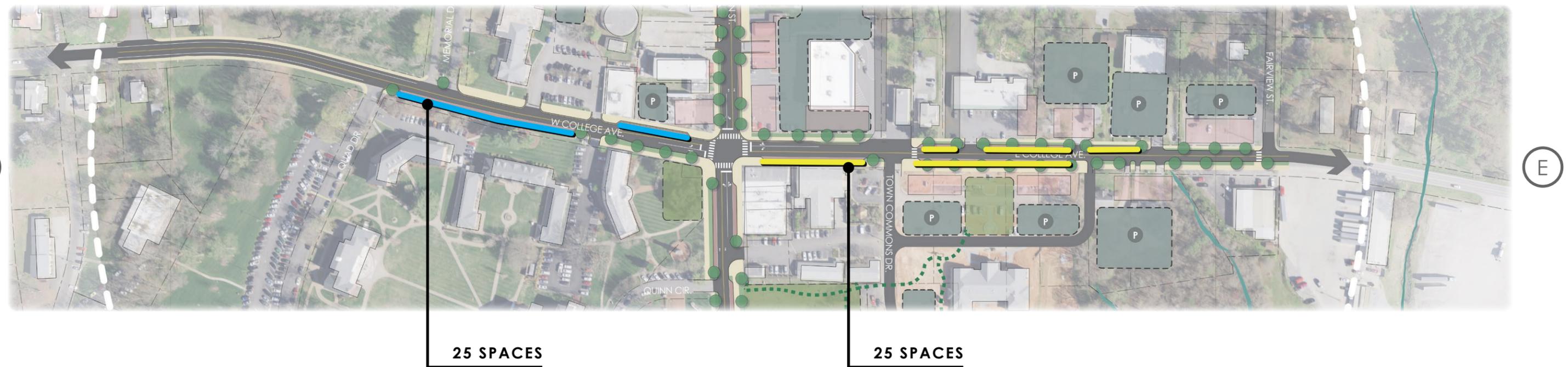


# PROPOSED ON-STREET PARKING

## Main Street Corridor



## College Avenue Corridor



## **PRIORITY PROJECTS**

As part of the planning process project consultants, Town staff, and the steering committee identified key inputs to establish priority corridors for near-term projects that will improve walk- and bike-ability and complete streets in Downtown Boiling Springs.

Criteria used to determine priority corridors included:

- 1. Ease of implementation:** Project can be completed in the next 5-10 years and requires minimal property acquisition, has opportunities to apply for funding, and does not rely on catalyst private development;
- 2. High-impact results:** Project generates impactful, lasting change in the 1/4-mile Downtown area in the near-term without needing additional private investment or other large land-use transformations;
- 3. Connections to destinations:** Project creates safe pedestrian and bicycle connections to popular destinations such as Downtown, Gardner-Webb University, existing trail systems, parks and recreation facilities; and,
- 4. Steering committee support:** Project was identified as a priority by the steering committee.

Using the aforementioned criteria, **South Main Street and East College Avenue are identified as priority corridors.** The priority corridors encompass the southeast quadrant of Downtown and are directly adjacent to the highest concentration of Town-owned land and green space in Boiling Springs. The southeast quadrant includes destinations such as the existing Town Park, the cemetery, Town Hall, the historical museum, the YMCA, and University buildings. There is also a variety of food outlets and other businesses along both priority corridors.

**Projects within priority corridors should be considered first for securing funding and implementation.** The following pages include detailed cut sheets for the top 4 projects within the priority corridors that should be explored for near-term implementation, including a brief overview of major features and design guidance, precedent imagery, and planning-level cost estimates.

## YMCA MULTI-USE PATH

### MAJOR FEATURES

- Length: approx. 1.3 miles
- 10' paved trail
- Wayfinding signage and pedestrian-scale lighting
- Public art

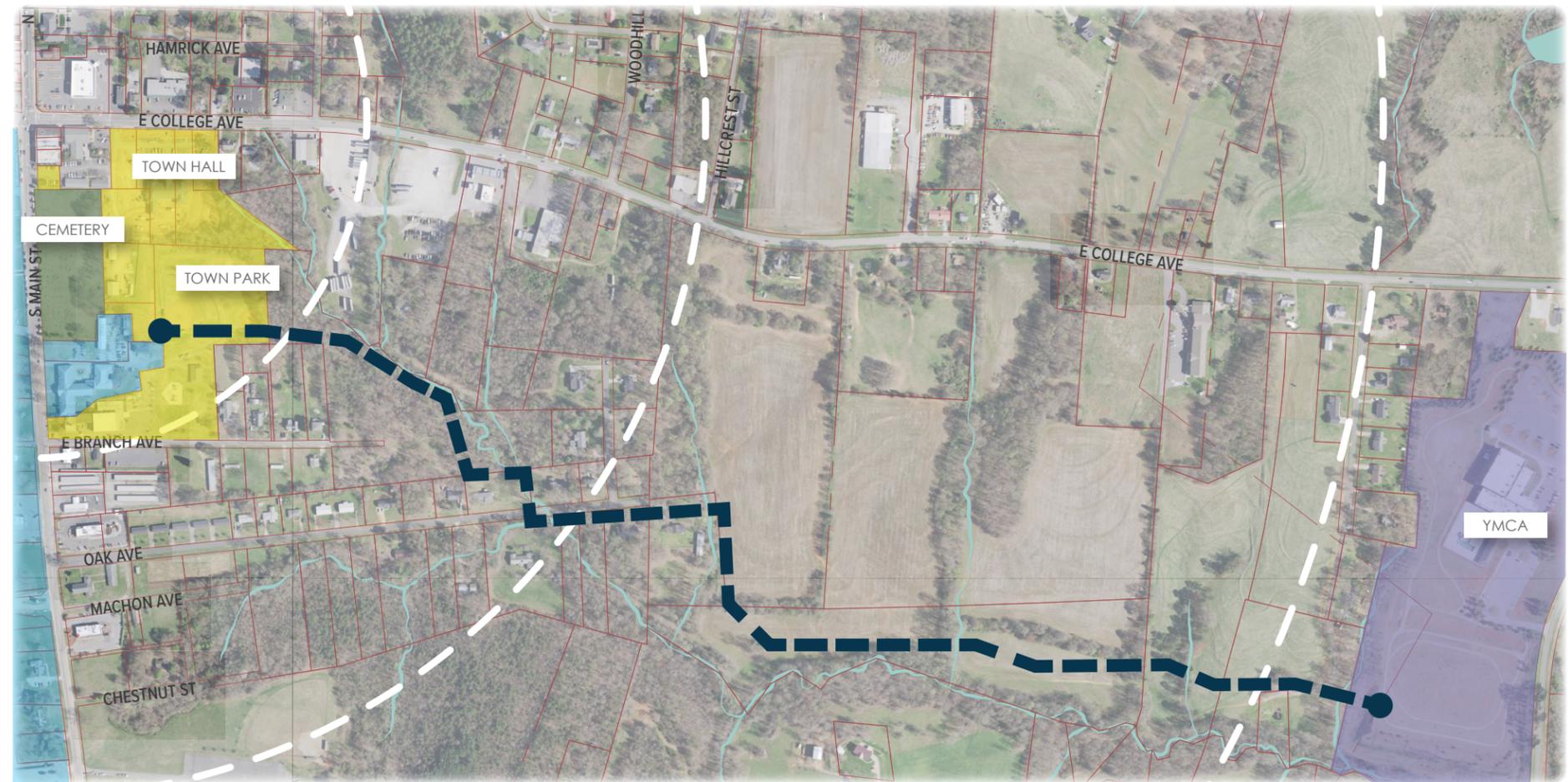
### GUIDANCE

- A 3-6' minimum shoulder width, or green zone (NACTO), should be provided on each side of the path and should be kept clear of vertical elements or obstructions.

Multi-use paths offer network connectivity opportunities beyond that of the roadway network for bicyclists, pedestrians, skaters, wheelchair users, joggers, and other users. These facilities can provide a low-stress experience and are often located in parks, along rivers, beaches, and in greenbelts or utility corridors (active or abandoned) where there are few conflicts with motorized vehicles. Multi-use paths provide a dedicated facility for users of all ages and abilities and support tourism while maintaining the community's distinctly rural character.

A new greenway operating in an off-street corridor independent from Main Street and College Avenue will provide a valuable transportation and recreational amenity for the community and a necessary connection to the local YMCA from the Downtown hub. **The proposed alignment begins at Town Commons Drive, reaching the northeast portion of Patrick Avenue.**

ITEM	COST
ITEM COST	\$ 448,650
2 YR INFLATION FACTOR (5%)	\$ 50,087
SUBTOTAL:	\$ 538,737
CONTINGENCIES (20%)	\$ 107,747
MINOR ITEMS (10%)	\$ 53,874
UTILITIES (1%)	\$ 5,387
SUBTOTAL:	\$ 705,745
E. + C. (10%)	\$ 70,575
<b>ESTIMATED PROJECT COST:</b>	<b>\$ 776,320</b>



Please note: This is not a design plan; precise locations and elements should be designed in accordance with engineering standards and NCDOT review.

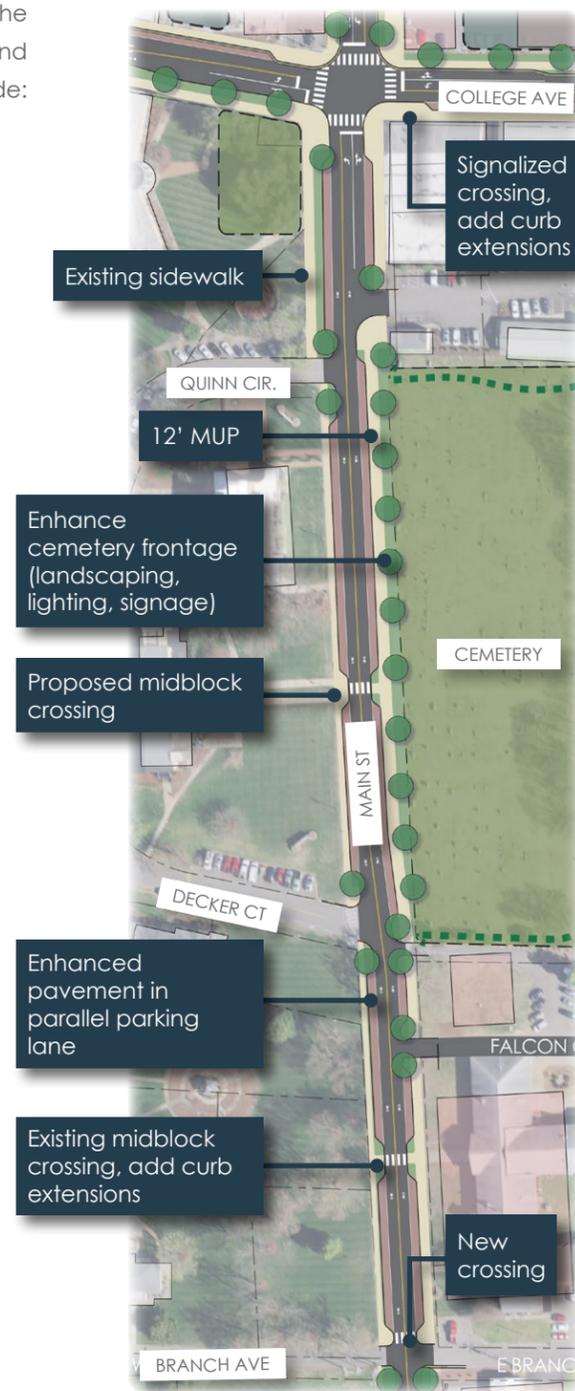
- Conceptual trail alignment
- Town-owned property
- Recreation center
- Gardner-Webb University

NOTE: PLEASE REFER TO THE APPENDIX FOR A COMPLETE BREAKDOWN OF THE COST ESTIMATION.

## SOUTH MAIN STREET STREETSCAPE

This segment of Main Street abuts Gardner-Webb University on the west and the historic Boiling Springs Cemetery on the east with a wide stretch of pavement in between. This project recommends reducing the existing travel lanes to 11' and moving in the east curb to narrow the road (see proposed cross section on page 17). Curb reconstruction will provide:

- Wider sidewalks in front of the existing businesses that may create opportunities for outdoor dining, street furniture, and sidewalk retail;
- Traffic calming for vehicles;
- 7' of landscaped green space in between the cemetery frontage and proposed 12' multi-use path;
- Curb extensions around on-street parking that can accommodate shade trees;
- Shorter crossing distance for pedestrians; and,
- A more welcoming and vibrant pedestrian environment that enhances the "downtown" feel.



### MAJOR FEATURES

- Length: approx. 0.25 miles (College Avenue to Branch Avenue)
- 12' paved sidepath
- Curb extensions for on-street recessed parking with enhanced pavement
- Shade trees and pedestrian-scale lighting
- Crossing improvements at 4 locations

### GUIDANCE

- Refer to the NCDOT Complete Streets Planning and Design Guide for guidance.

ITEM	COST
ITEM COST	\$676,600
2 YR INFLATION FACTOR (5%)	\$69,352
<b>SUBTOTAL:</b>	<b>\$745,952</b>
CONTINGENCIES (20%)	\$149,190
MINOR ITEMS (10%)	\$37,298
UTILITIES (1%)	\$208,866
<b>SUBTOTAL:</b>	<b>\$1,141,306</b>
E. + C. (10%)	\$114,131
<b>ESTIMATED PROJECT COST:</b>	<b>\$1,255,437</b>

NOTE: PLEASE REFER TO THE APPENDIX FOR A COMPLETE BREAKDOWN OF THE COST ESTIMATION.

# TOWN COMMONS DRIVE ROAD EXTENSION

## MAJOR FEATURES

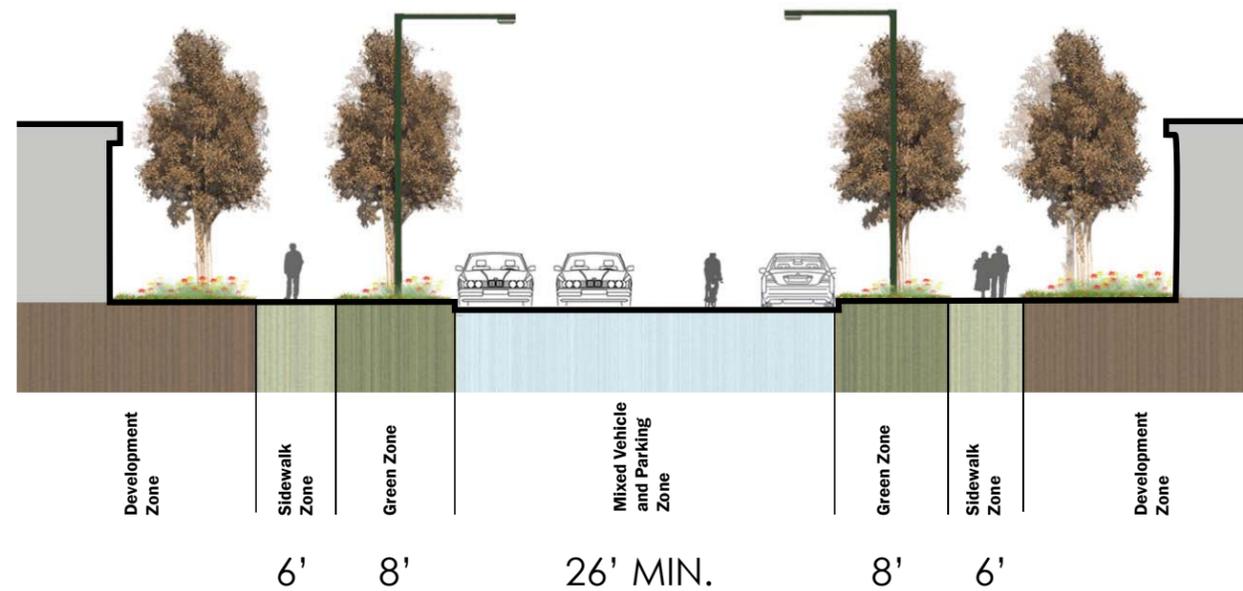
- Length: approx. 0.1 miles
- Low-speed low-volume street (sidewalk and sharrows)
- Wayfinding signage and pedestrian-scale lighting

## GUIDANCE

- Refer to the *NCDOT Complete Streets Planning and Design Guide* for guidance.

The extension of Town Commons Drive to Falcon Circle will improve connectivity to and visibility of valuable Boiling Springs amenities, such as the Town Hall, Town Park, and centralized public parking. The road connection will enhance the street grid system traditionally found in downtown areas, offering more route options for cyclists, pedestrians, and vehicles.

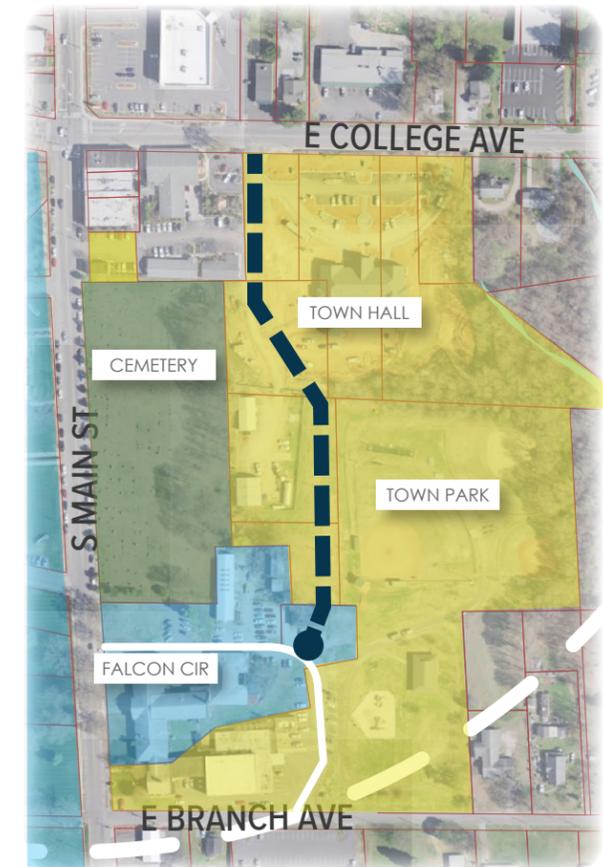
The proposed cross section for the street extension references the Local Office/Commercial/Industrial standard found in the *NCDOT Complete Street Planning and Design Guide*. Recommendations for this project include a mixed travel area, accommodating low-speed low-volume vehicular and bicycle traffic with parking on one side. Pavement width should be a minimum of 26' plus curb and gutter. Wayfinding signage, lighting, and shade trees along proposed sidewalks will encourage pedestrian activity in this area and contribute to a more vibrant and navigable walking environment.



(ABOVE) PROPOSED CROSS SECTION FROM NCDOT COMPLETE STREETS PLANNING AND DESIGN GUIDELINES.

ITEM	COST
ITEM COST	\$ 462,750
2 YR INFLATION FACTOR (5%)	\$ 47,432
SUBTOTAL:	\$ 510,182
CONTINGENCIES (20%)	\$ 102,036
MINOR ITEMS (10%)	\$ 25,509
UTILITIES (1%)	\$ 102,036
SUBTOTAL:	\$ 739,764
E. + C. (10%)	\$ 73,976
<b>ESTIMATED PROJECT COST:</b>	<b>\$ 813,740</b>

NOTE: PLEASE REFER TO THE APPENDIX FOR A COMPLETE BREAKDOWN OF THE COST ESTIMATION.



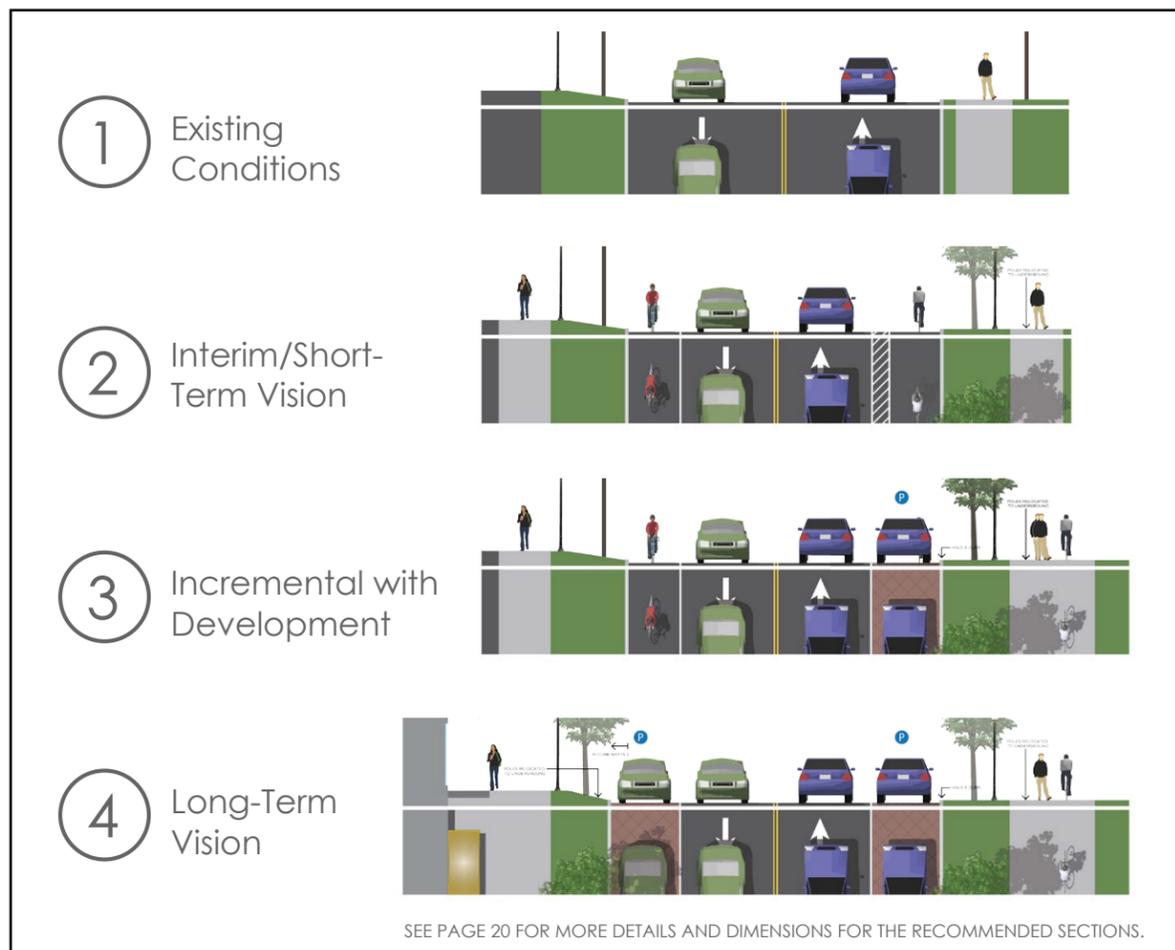
- Conceptual road alignment
- Town-owned property
- Gardner-Webb University

Please note: This is not a design plan; precise locations and elements should be designed in accordance with engineering standards and NCDOT review.

## EAST COLLEGE AVENUE STREETScape

Interim recommendations for East College Avenue have been identified as a priority for near-term implementation in Boiling Springs. This segment currently has two 18' travel lanes; pavement can be redistributed with paint to drastically improve the overall 'feel' of the streetscape and balance the competing needs of bicycle connectivity, pedestrian comfort, and parking availability. Sidewalks buffered with a wide planting strip are also recommended on both sides. The sections below illustrate the proposed cross section with a phased approach to reach the aspirational vision.

The interim street transformation sets College Avenue up to be adaptable to incremental future growth in Downtown. As private development occurs, the bike lanes can transition to on-street parking lanes to accommodate additional visitors and potential customers. The south side bike lane is expected to transition to parking first, due to its close proximity to destinations that are not expected to redevelop (such as Town Hall, the Town Park, and University-owned property). When this transition occurs, the southern sidewalk should be widened to a 10' sidepath before introducing bicycle traffic.



### Long-Term Vision



## MAJOR FEATURES

- Length: approx. 0.25 miles (Main Street to Fairview Street)
- Fill sidewalk gaps
- Redistribution of pavement width (paint)
- Shade trees
- Long-term only: curb extensions for recessed parking, sidewalk widening, curb relocation, enhanced pavement, crossing improvements at 3 locations

## GUIDANCE

- Refer to the *NCDOT Complete Streets Planning and Design Guide* for guidance.

ITEM	COST
ITEM COST	\$256,600
2 YR INFLATION FACTOR (5%)	\$26,302
SUBTOTAL:	\$282,902
CONTINGENCIES (20%)	\$56,580
MINOR ITEMS (10%)	\$28,290
UTILITIES (1%)	\$141,451
SUBTOTAL:	\$509,223
E. + C. (10%)	\$50,922
<b>ESTIMATED PROJECT COST:</b>	<b>\$560,145</b>

NOTE: PLEASE REFER TO THE APPENDIX FOR A COMPLETE BREAKDOWN OF THE COST ESTIMATION.

## CROSSING RECOMMENDATIONS

There are 16 pedestrian crossings identified for improvements in this plan, half of them located within the 1/4-mile radius of downtown. The following graphics depict the crossing upgrades and intersection improvements along Main Street and College Avenue within the 1/4-mile radius. See the reference diagram and table on the following page for specifics on the intersection treatments for each crossing within the downtown area. Please refer to the Network Recommendations map (page 12) for locations for other crossing improvements.

**(A.)** Main + Green/Bethel



**(B.)** Main + Woodland



**(C.)** Main + Quinn/Decker (midblock)



**(D.)** Main + Falcon (midblock)



**(E.)** Main + Branch



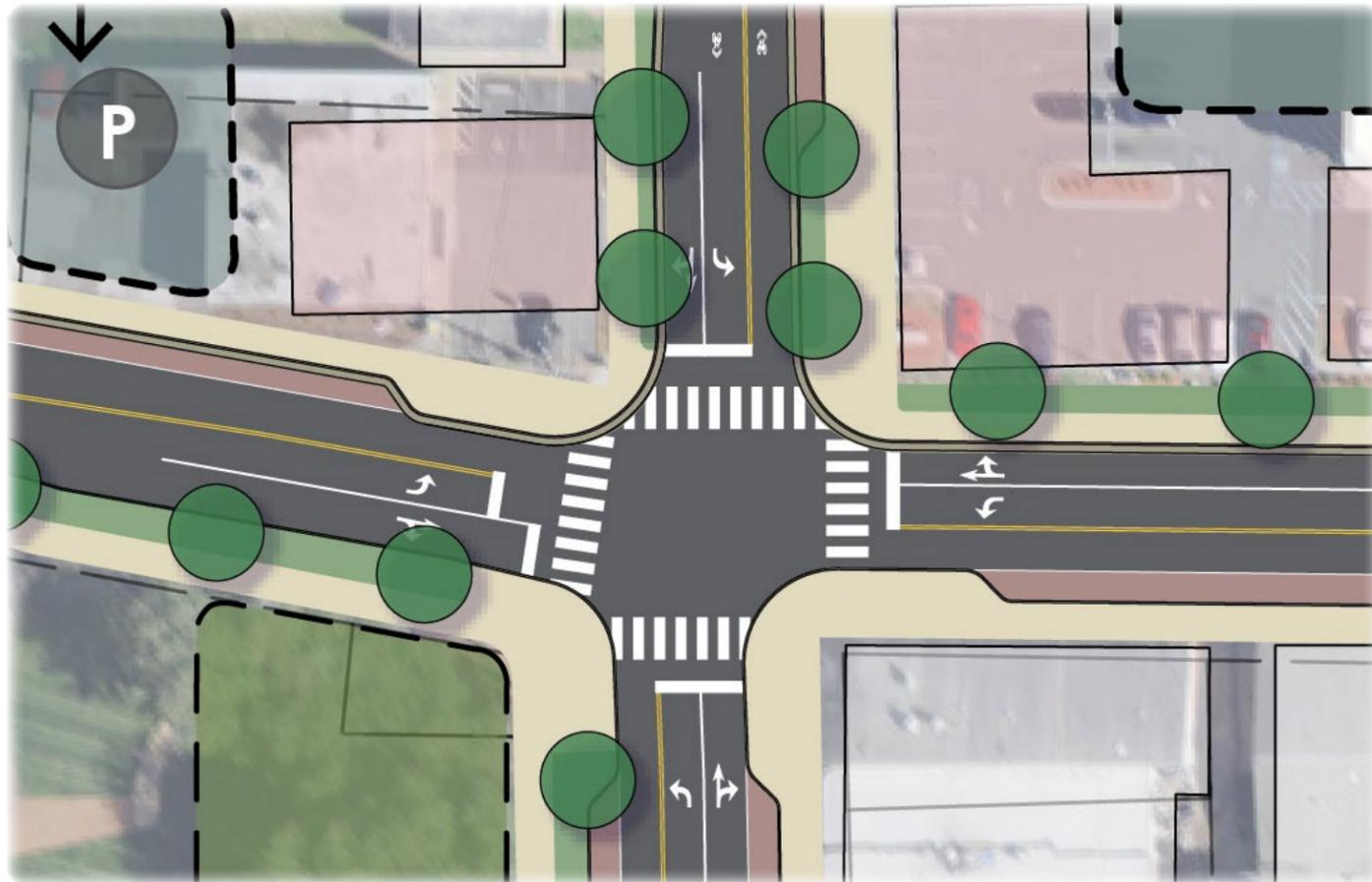
### Other Main St Crossing Improvement

#### Locations (north to south):

- Main + Homestead
- Main + Gordon/Holland (midblock)
- Main + Stadium
- Main + Patrick

## CROSSING RECOMMENDATIONS (CONT.)

F. Main Street + College Avenue (signalized)



**EXISTING**

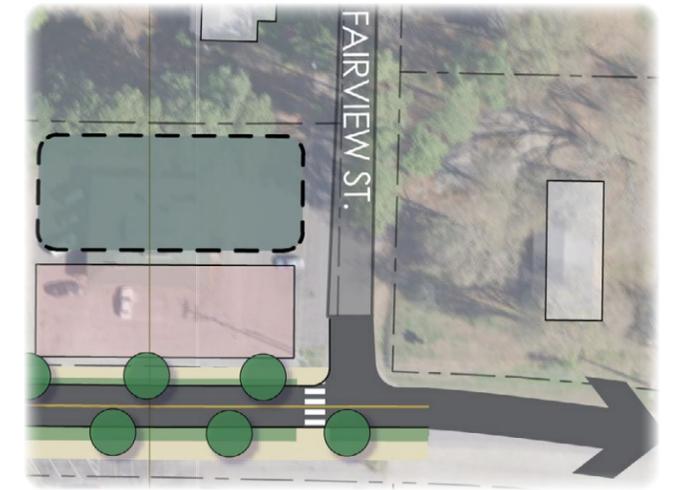
Main Street and College Avenue is the only signalized intersection within the study area and functions as the center-most point of Downtown. The intersection will be upgraded with sidewalk bulb-outs to reduce curb radii and provide shorter crossing distances for pedestrians. Other recommendations include marked high-visibility crosswalks at all 4 sides, pedestrian heads with improved signal timing that prioritizes pedestrian crossings, and pedestrian-scale lighting.

Intersection improvements could be implemented concurrently with the Main Street streetscape improvements (see Priority Project cutsheet, page 26).

G. College + Town Commons



H. College + Fairview



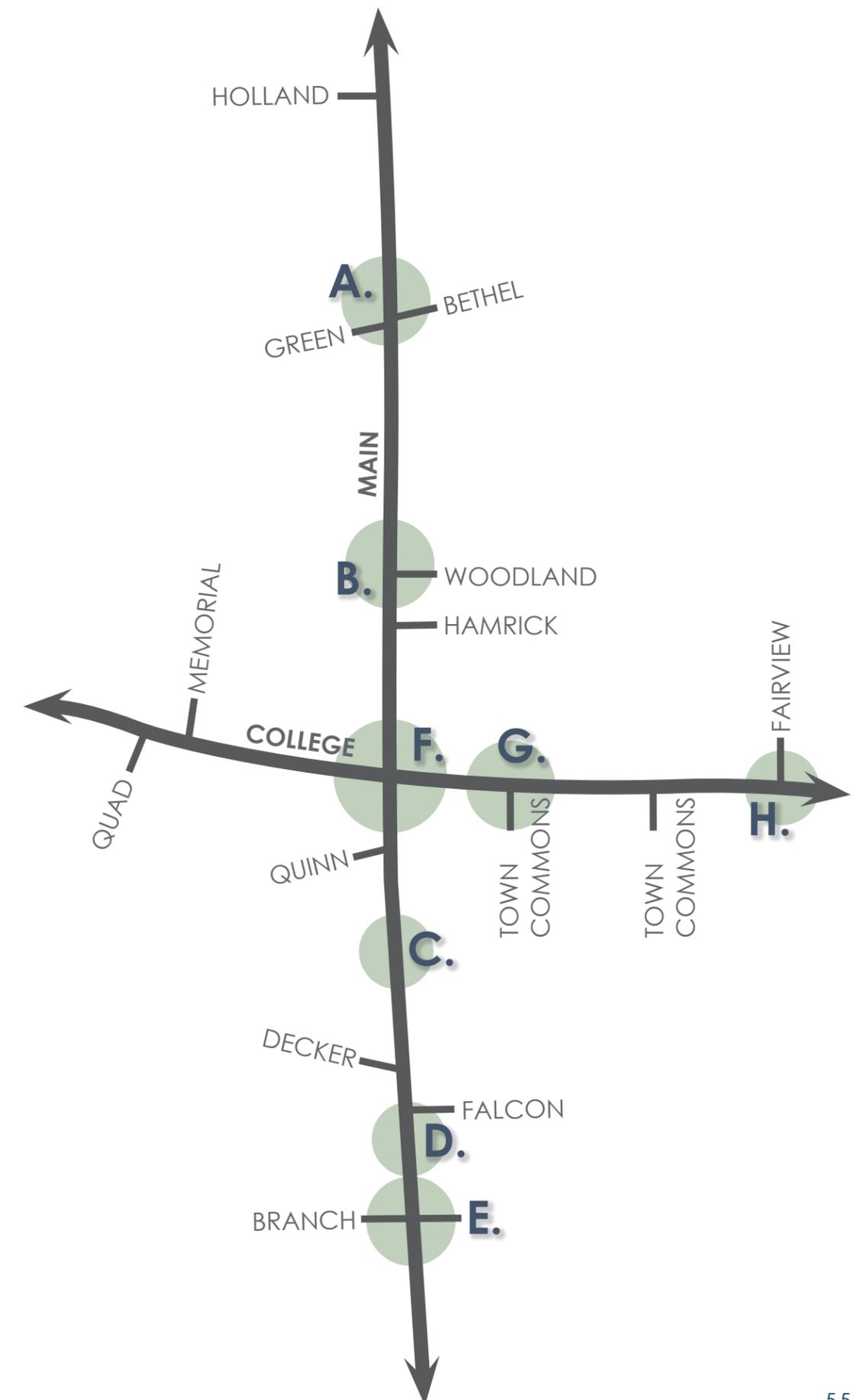
Other College Ave Crossing Improvement Locations (west to east):

- College + Creekside
- College + College Manor
- College + Hillcrest

CROSSROAD	EXISTING CONDITIONS	RECOMMENDED CROSSING UPGRADES
A. Main + Green/Bethel	--	Add median refuge island + crosswalk
B. Main + Woodland	--	Add sidewalk bulb-out + crosswalk
C. Main + Quinn/Decker (midblock)	--	Add sidewalk bulb-out + crosswalk
D. Main + Falcon (midblock)	Crosswalk + curb ramp	Widen raised sidewalk bulb-out
E. Main + Branch	--	Add sidewalk bulb-out + crosswalk
F. Main + College	Signalized (4-way) with crosswalks	Add pedestrian signals, sidewalk bulb-outs, crosswalks
G. College + Town Commons	--	Add sidewalk bulb-out + crosswalk
H. College + Fairview	--	Curb ramps + crosswalk

**OTHER CROSSING IMPROVEMENT LOCATIONS:**

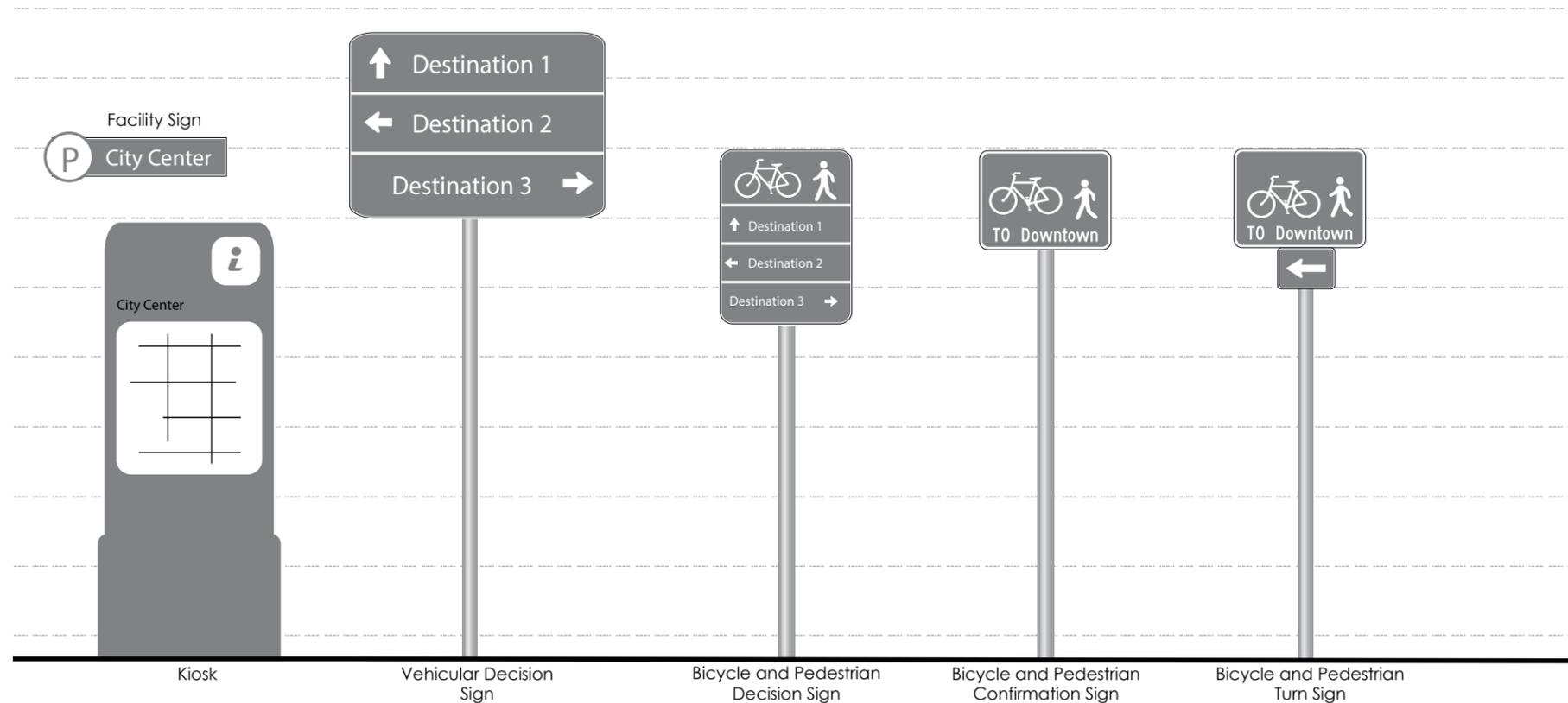
Main + Homestead	--	Roundabout, add median refuge islands + crosswalks
Main + Gordon/Holland (midblock)	--	Add median refuge island + crosswalk
Main + Stadium	Crosswalks across Stadium	Curb ramps + crosswalk
Main + Patrick	--	Roundabout, add crosswalks
Rockford + College Farm	--	Roundabout, add median refuge islands + crosswalks
College + College Manor	--	Add median refuge island + crosswalk
College + Creekside	--	Add median refuge island + crosswalk
College + Hillcrest	--	Curb ramps + crosswalk



## WAYFINDING

A community wayfinding plan and consistent signage design will reflect the character of Boiling Springs and reinforce the value of the assets available to visitors and residents. Signage helps orient pedestrians and bicyclists and motorists to key destinations and assists with distance tracking. Investing in a permanent wayfinding signage program is an important step in creating a more cohesive, welcoming, and accessible pedestrian and bicycling environment in Boiling Springs.

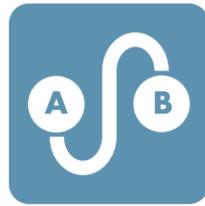
A consistent approach to selecting and signing along the corridor is necessary where limited space is available for information and a multitude of potential points of interest exist. The most appropriate location for wayfinding is at major intersections and decision points. Consideration of the context and natural landscape is of utmost importance; signage design should reflect the local flavor of the community with colors and materials that are similar to what already exists Downtown.



(ABOVE) STANDARD WAYFINDING SIGN PACKAGE.

## WAYFINDING SIGN CONCEPT

### Guiding Principles



#### 1. CONNECT PLACES

Facilitate travel between destinations and provide guidance to new destinations.



#### 2. KEEP INFORMATION SIMPLE

Present information simply, using clear fonts and simple designs, so that it can be understood quickly.



#### 3. MAINTAIN MOTION

Be legible and visible for people moving so that they can read the signage without stopping.



#### 4. BE PREDICTABLE

Standardize the placement and design of signs so that patterns are established and the signage becomes predictable.



#### 5. PROMOTE ACTIVE TRAVEL

Encourage increased rates of active transportation by helping people to realize they can use the bikeway and pedestrian network to access the places they want to go.

Wayfinding signage should be simple and easy to read. The concept below builds off of existing University signage and includes the quintessential orange flame of the Gardner-Webb logo to quickly let users know that they are near the campus. Integrated with the existing Town branding and the approximate time and distance to local destinations, signage directs pedestrians and bicyclists into Downtown. This design could be expanded into more signage types including public parking signs, mile markers, information kiosks, trailhead markers, and educational or historical signage. The Town is currently working on a rebranding effort and wayfinding signage should incorporate elements of this effort and integrate with University marketing efforts.



## APPENDIX



**CONCEPTUAL ESTIMATE**

NC License #P-1301

DESCRIPTION AND LOCATION: YMCA Trail Connection  
Boiling Springs, NC  
 COUNTY: Cleveland

ITEM NO.			ITEM DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	AMOUNT
LINE NO.	DESC. NO.	SECT. NO.					
<b>ROADWAY ITEMS</b>							
	0000100000-N	800	MOBILIZATION	1	LS	\$34,900.00	\$34,900.00
	0000400000-N	801	CONSTRUCTION SURVEYING	1	LS	\$7,500.00	\$7,500.00
	0043000000-N	226	GRADING	1	LS	\$105,800.00	\$105,800.00
	0354000000-E	310	**** RC PIPE CULVERTS, CLASS *****	200	LF	\$100.00	\$20,000.00
	1121000000-E	520	AGGREGATE BASE COURSE	2110	TON	\$35.00	\$73,850.00
	1523000000-E	610	ASPHALT CONC SURFACE COURSE, TYPE S9.5C	650	TON	\$200.00	\$130,000.00
	2591000000-E	848	4" CONCRETE SIDEWALK	560	SY	\$40.00	\$22,400.00
	4399000000-N	1105	TEMPORARY TRAFFIC CONTROL	1	LS	\$11,000.00	\$11,000.00
	6000000000-E	1605	TEMPORARY SILT FENCE	5600	LF	\$3.00	\$16,800.00
	6029000000-E	SP	SAFETY FENCE	200	LF	\$50.00	\$10,000.00
	6084000000-E	1660	SEEDING & MULCHING	1.6	ACR	\$4,000.00	\$6,400.00
			BOARDWALK	100	LF	\$500.00	\$50,000.00

	ITEM COST	\$488,650.00
INFLATION FACTOR	2 YEARS 5%	\$50,086.63
	CONSTRUCTION COST (2022)	\$538,736.63
CONTINGENCIES	20.0%	\$107,747.33
MINOR ITEMS	10.0%	\$53,873.66
UTILITIES	1.0%	\$5,387.37
RIGHTS OF WAY	0.0%	\$0.00
	ESTIMATED CONTRACT COST (2022)	\$705,744.98
	E. & C. (10%)	\$70,574.50
	CONSTRUCTION COST (2022)	\$776,319.48

NOTE: \_\_\_\_\_

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COMPUTED BY DEJ  
 DATE 12/16/2019



**CONCEPTUAL ESTIMATE**

NC License #P-1301

DESCRIPTION AND LOCATION: S Main Street  
Boiling Springs, NC  
 COUNTY: Cleveland

ITEM NO.			ITEM DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	AMOUNT
LINE NO.	DESC. NO.	SECT. NO.					
	0000100000-N	800	MOBILIZATION	1	LS	\$47,900.00	\$47,900.00
	0000400000-N	801	CONSTRUCTION SURVEYING	1	LS	\$6,000.00	\$6,000.00
	0043000000-N	226	GRADING	1	LS	\$62,400.00	\$62,400.00
	1297000000-E	607	MILLING ASPHALT PAVEMENT, 1-1/2" DEPTH	5390	SY	\$10.00	\$53,900.00
	1523000000-E	610	ASPHALT CONC SURFACE COURSE, TYPE S9.5C	260	TON	\$200.00	\$52,000.00
	2352000000-N	840	FRAME WITH GRATE, STD 840.****	2	EA	\$700.00	\$1,400.00
	2535000000-E	846	***X*** CONCRETE CURB	1400	LF	\$25.00	\$35,000.00
	2591000000-E	848	4" CONCRETE SIDEWALK	1600	SY	\$50.00	\$80,000.00
	2605000000-N	848	CONCRETE CURB RAMP	14	EA	\$3,000.00	\$42,000.00
	4399000000-N	1105	TEMPORARY TRAFFIC CONTROL	1	LS	\$24,000.00	\$24,000.00
	5672000000-N	1515	RELOCATE FIRE HYDRANT	2	EA	\$4,000.00	\$8,000.00
	6000000000-E	1605	TEMPORARY SILT FENCE	2000	LF	\$3.00	\$6,000.00
	6084000000-E	1660	SEEDING & MULCHING	0.3	ACR	\$4,000.00	\$1,200.00
	6695000000-E	SP	GENERIC PLANTING ITEM STREET TREES	21	EA	\$800.00	\$16,800.00
			GENERIC PAVEMENT WITH SURFACE TREATMENT	12000	SF	\$20.00	\$240,000.00

	ITEM COST	\$676,600.00
INFLATION FACTOR	2 YEARS 5%	\$69,351.50
	CONSTRUCTION COST (2022)	\$745,951.50
CONTINGENCIES	20.0%	\$149,190.30
MINOR ITEMS	5.0%	\$37,297.58
UTILITIES	28.0%	\$208,866.42
RIGHTS OF WAY	0.0%	\$0.00
	ESTIMATED CONTRACT COST (2022)	\$1,141,305.80
	E. & C. (10%)	\$114,130.58
	CONSTRUCTION COST (2022)	\$1,255,436.37

NOTE: \_\_\_\_\_

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COMPUTED BY DEJ  
 DATE 12/16/2019



### CONCEPTUAL ESTIMATE

NC License #P-1301

DESCRIPTION AND LOCATION: Town Commons Drive Extension  
Boiling Springs, NC  
COUNTY: Cleveland

ITEM NO.			ITEM DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	AMOUNT
LINE NO.	DESC. NO.	SECT. NO.					
<b>ROADWAY ITEMS</b>							
	0000100000-N	800	MOBILIZATION	1	LS	\$33,400.00	\$33,400.00
	0000400000-N	801	CONSTRUCTION SURVEYING	1	LS	\$5,000.00	\$5,000.00
	0043000000-N	226	GRADING	1	LS	\$52,950.00	\$52,950.00
	1491000000-E	610	ASPHALT CONC BASE COURSE, TYPE B25.0C	560	TON	\$175.00	\$98,000.00
	1503000000-E	610	ASPHALT CONC INTERMEDIATE COURSE, TYPE I19.0C	370	TON	\$200.00	\$74,000.00
	1523000000-E	610	ASPHALT CONC SURFACE COURSE, TYPE S9.5C	270	TON	\$200.00	\$54,000.00
	2549000000-E	846	2'-6" CONCRETE CURB & GUTTER	1100	LF	\$35.00	\$38,500.00
	2591000000-E	848	4" CONCRETE SIDEWALK	1470	SY	\$50.00	\$73,500.00
	4399000000-N	1105	TEMPORARY TRAFFIC CONTROL	1	LS	\$8,000.00	\$8,000.00
	6000000000-E	1605	TEMPORARY SILT FENCE	6200	LF	\$3.00	\$18,600.00
	6084000000-E	1660	SEEDING & MULCHING	1.7	ACR	\$4,000.00	\$6,800.00

	ITEM COST	\$462,750.00
INFLATION FACTOR	2 YEARS 5%	\$47,431.88
	CONSTRUCTION COST (2022)	\$510,181.88
CONTINGENCIES	20.0%	\$102,036.38
MINOR ITEMS	5.0%	\$25,509.09
UTILITIES	20.0%	\$102,036.38
RIGHTS OF WAY	0.0%	\$0.00
	ESTIMATED CONTRACT COST (2022)	\$739,763.72
	E. & C. (10%)	\$73,976.37
	CONSTRUCTION COST (2022)	\$813,740.09

NOTE: \_\_\_\_\_

COMPUTED BY DEJ

DATE 12/16/2019



### CONCEPTUAL ESTIMATE

NC License #P-1301

DESCRIPTION AND LOCATION: E College Street  
Boiling Springs, NC  
COUNTY: Cleveland

ITEM NO.			ITEM DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	AMOUNT
LINE NO.	DESC. NO.	SECT. NO.					
	0000100000-N	800	MOBILIZATION	1	LS	\$17,200.00	\$17,200.00
	0000400000-N	801	CONSTRUCTION SURVEYING	1	LS	\$5,000.00	\$5,000.00
	0043000000-N	226	GRADING	1	LS	\$19,800.00	\$19,800.00
	1297000000-E	607	MILLING ASPHALT PAVEMENT, 1-1/2" DEPTH	4410	SY	\$10.00	\$44,100.00
	1523000000-E	610	ASPHALT CONC SURFACE COURSE, TYPE S9.5C	340	TON	\$200.00	\$68,000.00
	2374000000-N	840	FRAME WITH GRATE & HOOD, STD 840.03, TYPE **	3	EA	\$1,100.00	\$3,300.00
	2535000000-E	846	***X *** CONCRETE CURB	950	LF	\$25.00	\$23,750.00
	2591000000-E	848	4" CONCRETE SIDEWALK	500	SY	\$50.00	\$25,000.00
	2600000000-N	SP	RETROFIT EXISTING CURB RAMP	1	EA	\$1,000.00	\$1,000.00
	2605000000-N	848	CONCRETE CURB RAMP	3	EA	\$3,000.00	\$9,000.00
	4399000000-N	1105	TEMPORARY TRAFFIC CONTROL	1	LS	\$20,000.00	\$20,000.00
	5672000000-N	1515	RELOCATE FIRE HYDRANT	1	EA	\$4,000.00	\$4,000.00
	6012000000-E	1610	SEDIMENT CONTROL STONE	20	TON	\$150.00	\$3,000.00
	6042000000-E	1632	1/4" HARDWARE CLOTH	225	LF	\$10.00	\$2,250.00
	6084000000-E	1660	SEEDING & MULCHING	0.2	ACR	\$4,000.00	\$800.00
	6695000000-E	SP	GENERIC PLANTING ITEM STREET TREES	13	EA	\$800.00	\$10,400.00

	ITEM COST	\$256,600.00
INFLATION FACTOR	2 YEARS 5%	\$26,301.50
	CONSTRUCTION COST (2022)	\$282,901.50
CONTINGENCIES	20.0%	\$56,580.30
MINOR ITEMS	10.0%	\$28,290.15
UTILITIES	50.0%	\$141,450.75
RIGHTS OF WAY	0.0%	\$0.00
	ESTIMATED CONTRACT COST (2022)	\$509,222.70
	E. & C. (10%)	\$50,922.27
	CONSTRUCTION COST (2022)	\$560,144.97

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DATE 12/16/2019

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# BOILING SPRINGS | NC RECOMMENDATIONS

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Site Specific Opportunities

Executive Summary

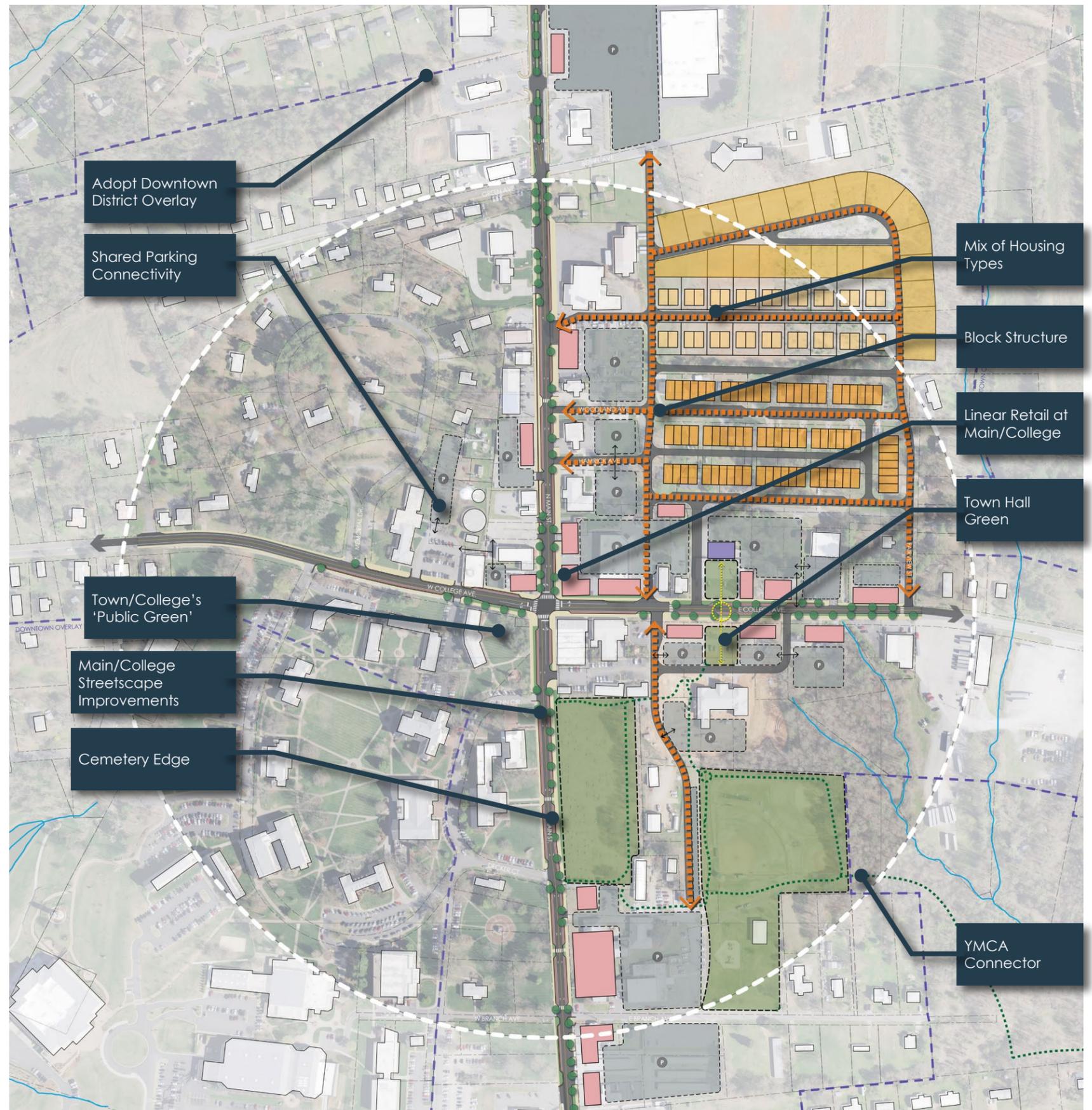
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## SITE SPECIFIC OPPORTUNITIES

BOILING SPRINGS | NC

Based on the due diligence, analysis, synthesis and collective recommendations provided within this document, the list below highlights particular "Site Specific Opportunities" that are projected to have the most immediate impacts for the surrounding community.

- **YMCA Connector** : Development and easement acquisition to create a multi-modal pedestrian path to YMCA from Downtown, utilizing the existing park's proximity.
- **Town Hall Green** : Redevelopment opportunity with programmed events
- **Main/College Streetscape Improvements** : Promoting walkable streets begins with improvements including widened sidewalks, dedicated bicycle lanes, reduced travel lanes, and enhanced crossings.
- **Cemetery Edge** : Improved circulation, enhance landscaping, branding, and maintenance
- **'Public Green' at southwest corner of Main/College** - Opportunity for a collaborative partnership with GWU for a potential amphitheater and usable public space.
- **Improve Vehicular Circulation & Block Structure** : Redevelopment and development should promote walkable streets, which is a result of shorter block structure, street-oriented buildings, and reduced curb cuts. Creating alleys and additional secondary roads along Hamrick Ave, Woodland Ave, Bethel Ave, and Oak Ave in response to the surrounding conditions will emphasize decrease traffic speeds and increase community traffic. Additionally, improving Town Commons Drive past the old Town Hall will improve circulation and provide additional pedestrian connections.
- **Shared Parking Connectivity** : Water Tower and Post Office parking lots are prime locations to create shared parking opportunities.
- **Post Office Site** : Redevelopment opportunity
- **Mix of Housing Types** : Infill opportunities exist in nearby residential neighborhoods, but more specifically, housing inventory along Hamrick Ave and Woodland Ave could redevelop into a multi-family or attached housing typology to increase density and increase housing availability.
- **Liner Retail at northeast corner of Main/College** : Redevelopment



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## EXECUTIVE SUMMARY

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BOILING SPRINGS | NC

**Master Plan (Short- & Long-term)** - The downtown planning efforts are focused on key initiatives that can advance economic, social and physical successes of a thriving downtown Boiling Springs within the next 5 to 10 years. By integrating improved circulation and access at key locations with the downtown framework, the impact that both micro- and macro- interventions will boost and improve the town's sense of place.

**Retail Revitalization** - Current residents note an existing lack of dining, shopping and small business within the downtown core area. Unlike large commercial stores that have succumbed to the complexities of on-line shopping, small businesses still thrive in downtown areas that encourage pedestrian walkability and reflect local town character. Expanding existing sidewalks, encouraging renovations of existing buildings, generating outdoor seating and providing residents with more public services and access to recreation will reinforce the "small town feel".

**Transportation, Circulation, and Parking** - Bridging broken vehicular and pedestrian connections will liven the downtown area. Adopting a transportation plan that widens existing sidewalks, promotes bicycle lanes, encourages on-street parking and diets road cross sections to slow traffic will enhance the pedestrian experience. Instituting a plan in conjunction with the proper agencies that is flexible to current development patterns will present more mixed-use opportunities.

**Mixed of Housing Typologies** - Charlotte, NC and the surrounding areas have experienced a large influx of housing and transportation needs over the last 15 to 20 years due to the increasing population growth of the larger metropolitan area. This expansion has affected surrounding small towns in a variety of ways, but an increasingly popular issue is the lack of housing diversity, specifically attached single family housing. Looking ahead, the Town must develop effective strategies that integrate required housing diversity initiatives that allow for the development of scalable housing and reduce the gaps of undeveloped or lower density development. Creative funding strategies could help the Town ensure strong efforts are made to advocate for the development of affordable housing.

1

YMCA  
Greenway  
Connector

2

Town Hall  
Green

3

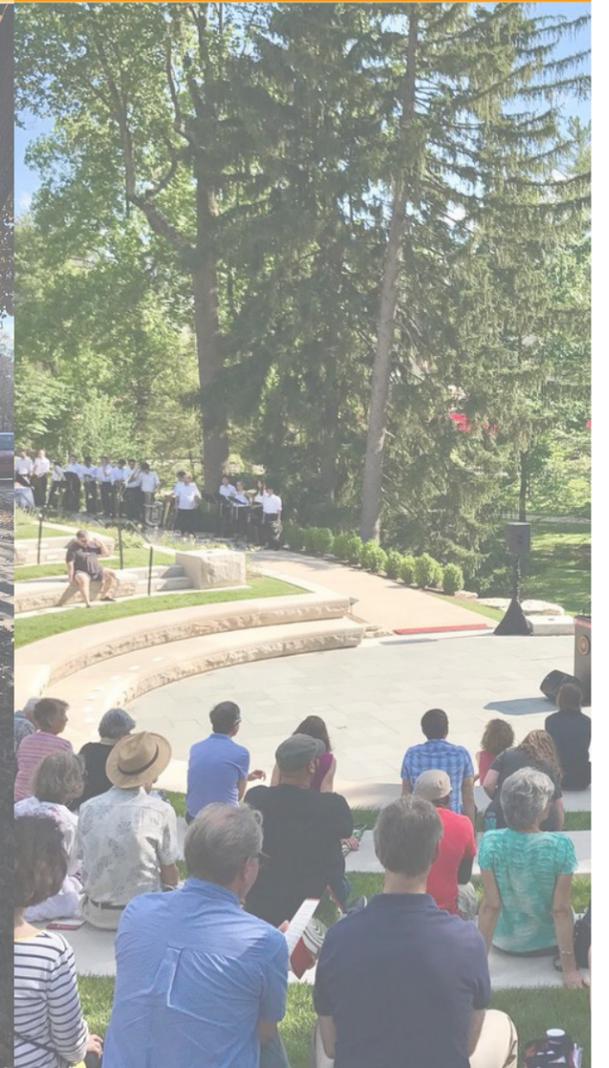
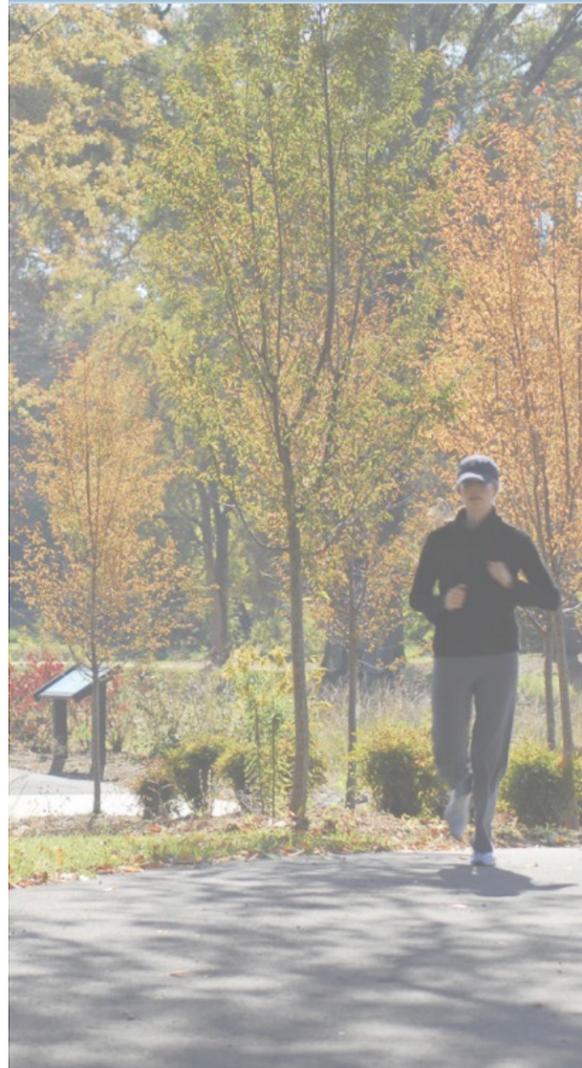
Main/College  
Streetscape  
Improvements

4

Cemetery  
Edge

5

Public Green at  
SW Corner of  
Main/College



TOP 10 IN 5 (1)

6

Improve Block Structure

7

Shared Parking Connectivity

8

Post Office Redevelopment

9

Mix of Housing Types

10

Liner Retail at NE corner of College/Main



TOP 10 IN 5 (2)



---

## DOWNTOWN MASTER PLAN

---

BOILING SPRINGS | NORTH CAROLINA

# ACKNOWLEDGMENTS

### MAYOR

Bill Elis

### TOWN COUNCIL

Mary Ruth Dixon (Mayor Pro Tempore)

Tommy C. Greene

Patrick Litton

Daniel Thomas

Marty L. Thomas

### TOWN MANAGER

Lucas A. Shires, MUD, PLA

### CONSULTANT TEAM

ColeJenest & Stone

Alta Planning + Design

### PLANNING BOARD

Buster Bryson

Bill Daves

Pat Hamrick

Ellen Humphries (Vice-Chair)

Wayne Johnson (Chair)

Chris Martin

Alan McWhirter

J.T. Scruggs

### STEERING COMMITTEE

Tammy Bass (GWU, Resident)

Wayne Johnson (Planning Board, Resident)

Jane Love (GCLMPO)

Chris Martin (Cleveland County, Planning Board, Resident)

Alan McWhirter (YMCA, Planning Board, Resident)

David Odom (Civil Engineering)

Julio Paredes (GCLMPO)

Dennis Terry (Landscape Architecture)

### TOWN SUPPORTING STAFF

Rhonda Allen (Finance Director)

Mike Gibert (Public Works Director)

Kim Greene (Town Clerk)

Nathan Phillips (Police Chief)

### GCLMPO & NCDOT Supporting Staff

Andy Bailey (NCDOT)

Dominique Boyd (NCDOT)

Randi Gates (GCLMPO)

Dean Ledbetter (NCDOT)

Jane Love (GCLMPO)

Julio Paredes (GCLMPO)

Brian Wert (NCDOT)

Daryl Vreeland (NCDOT)



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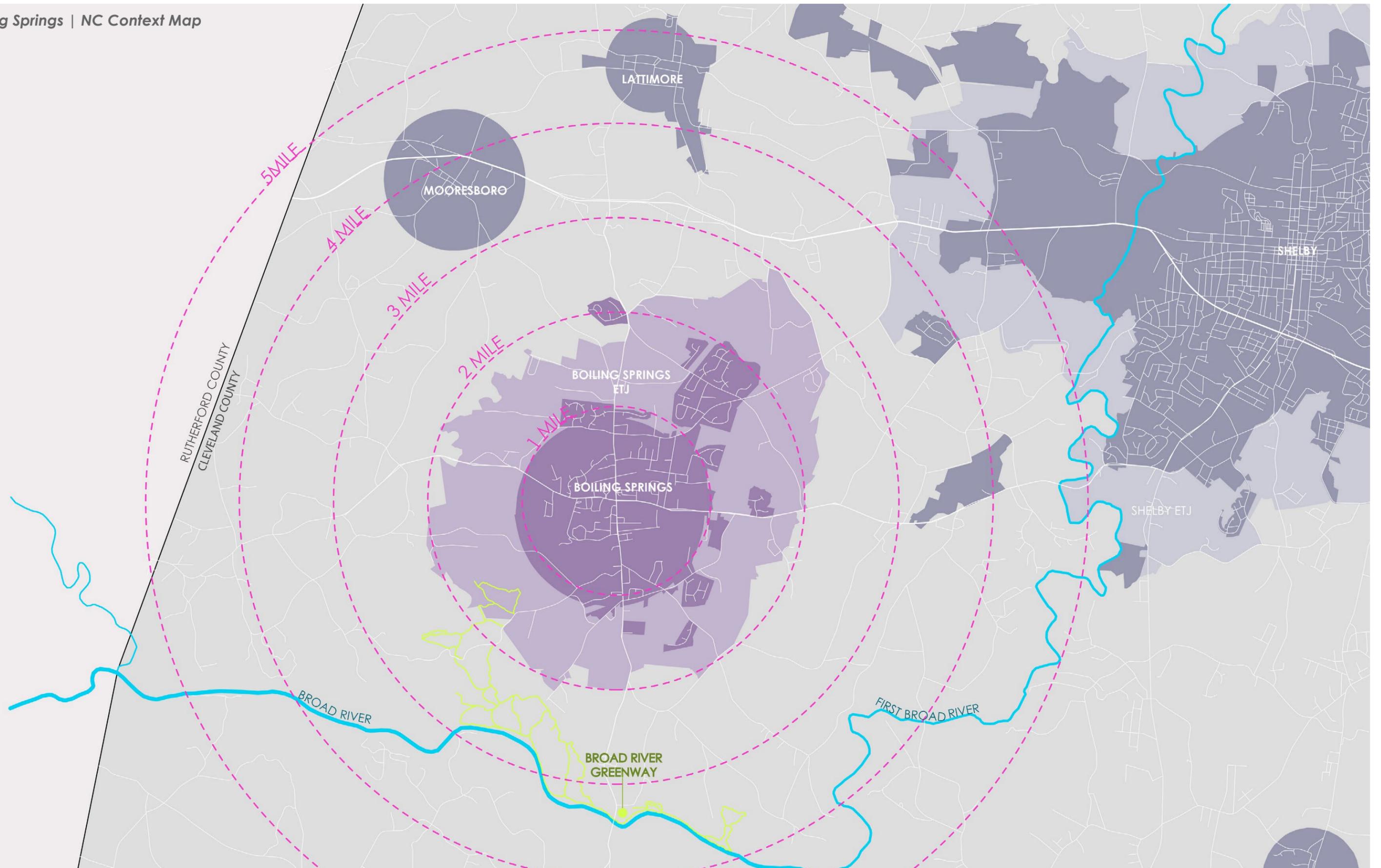
## DOWNTOWN MASTER PLAN

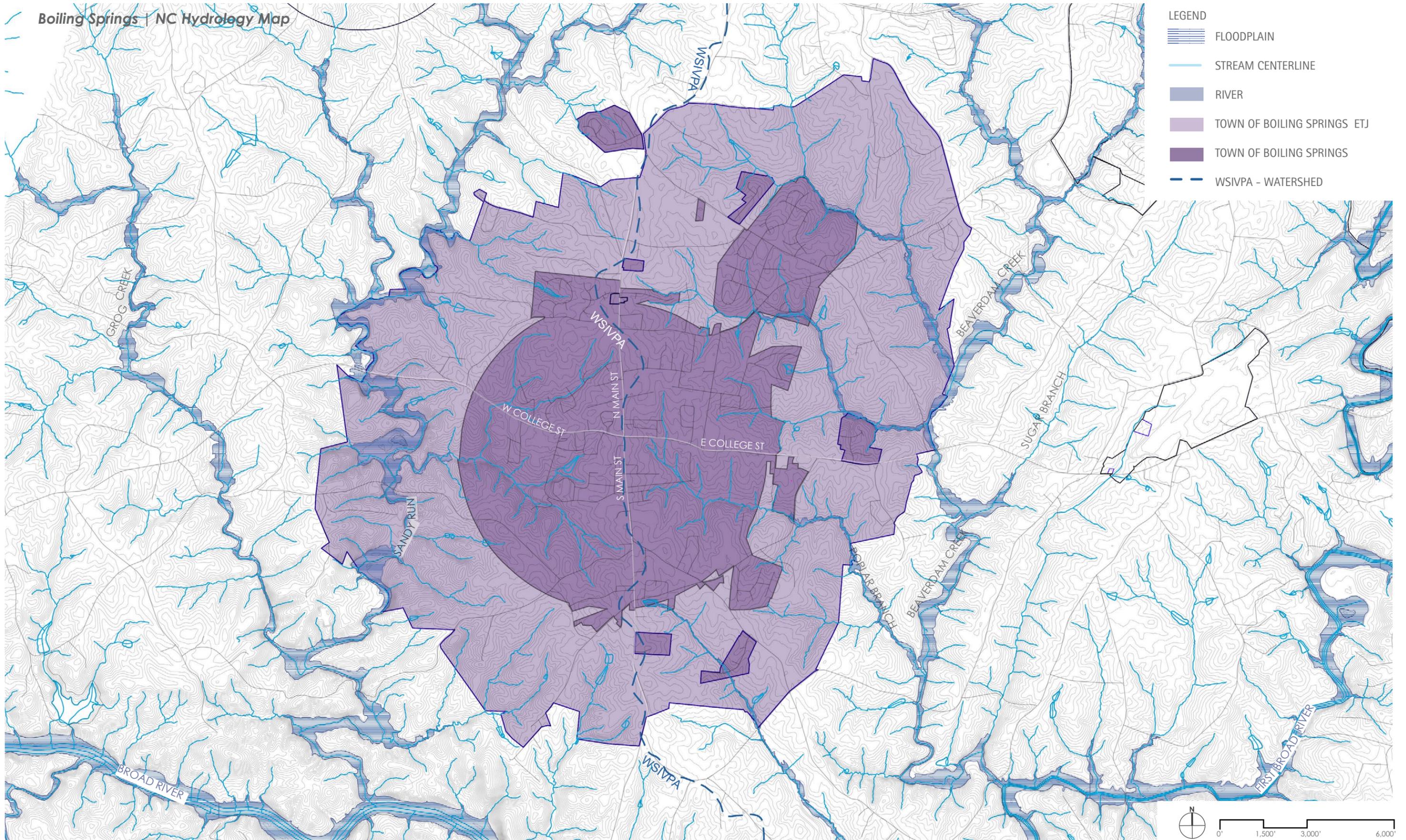
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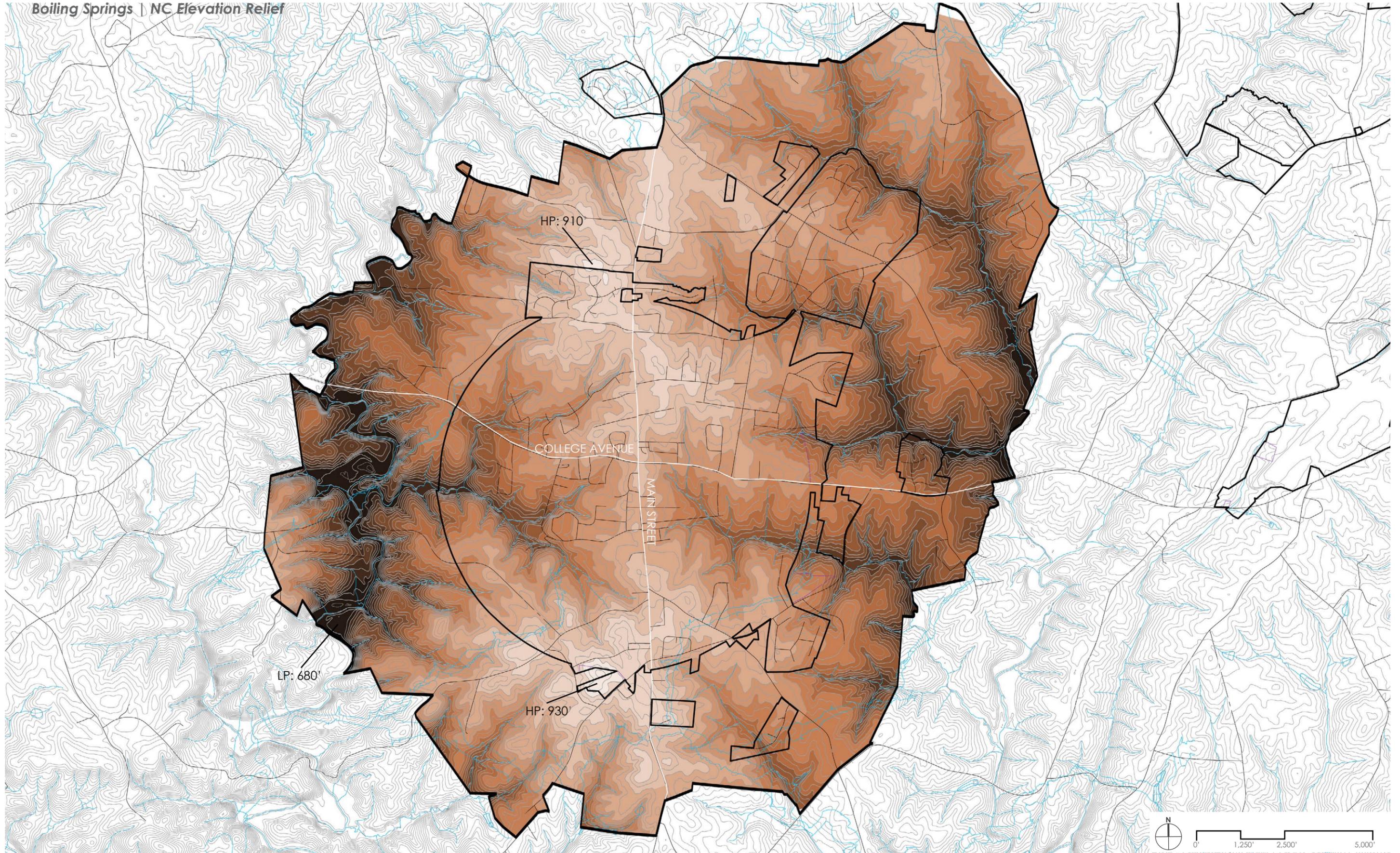
BOILING SPRINGS | NORTH CAROLINA

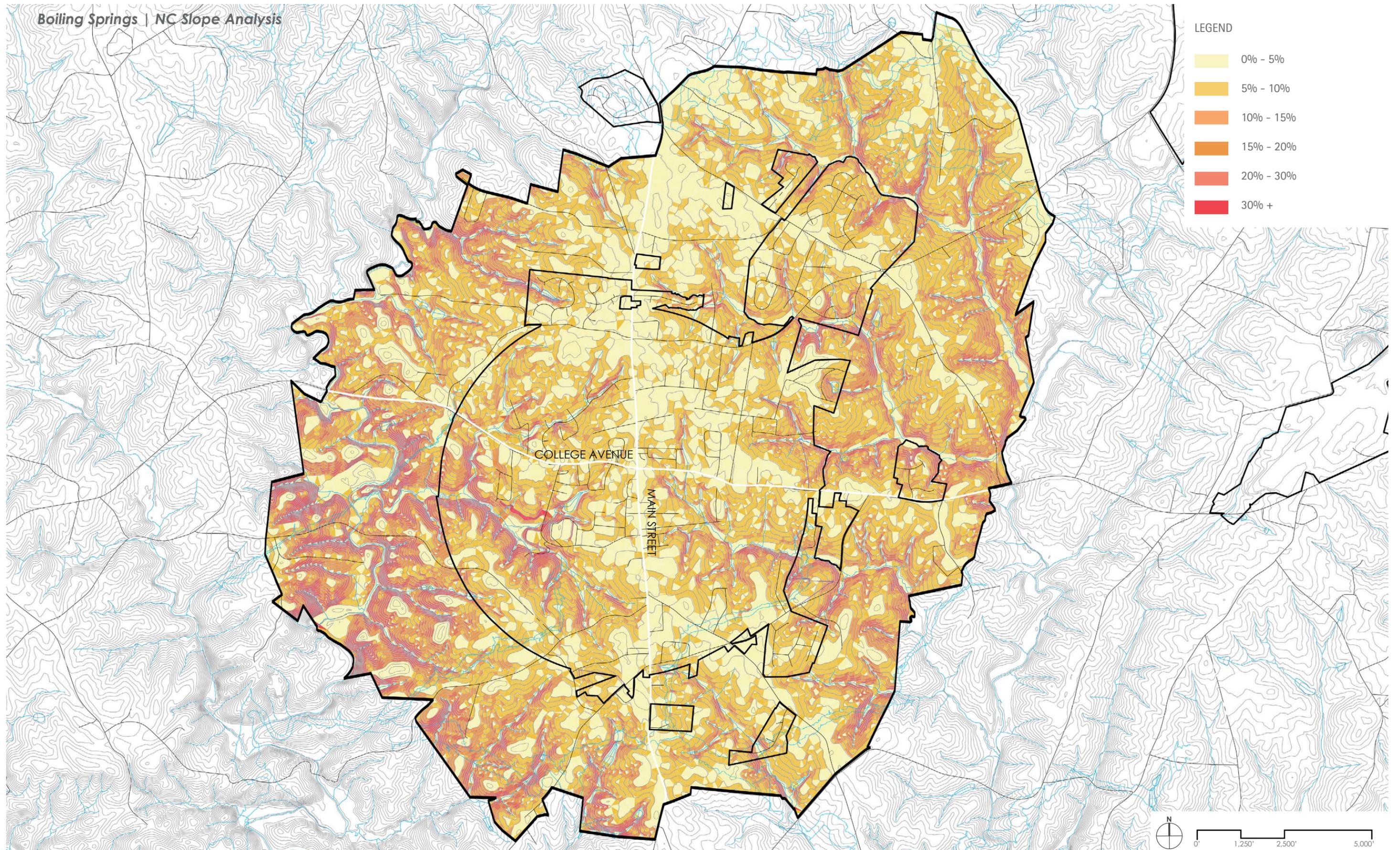
# APPENDIX

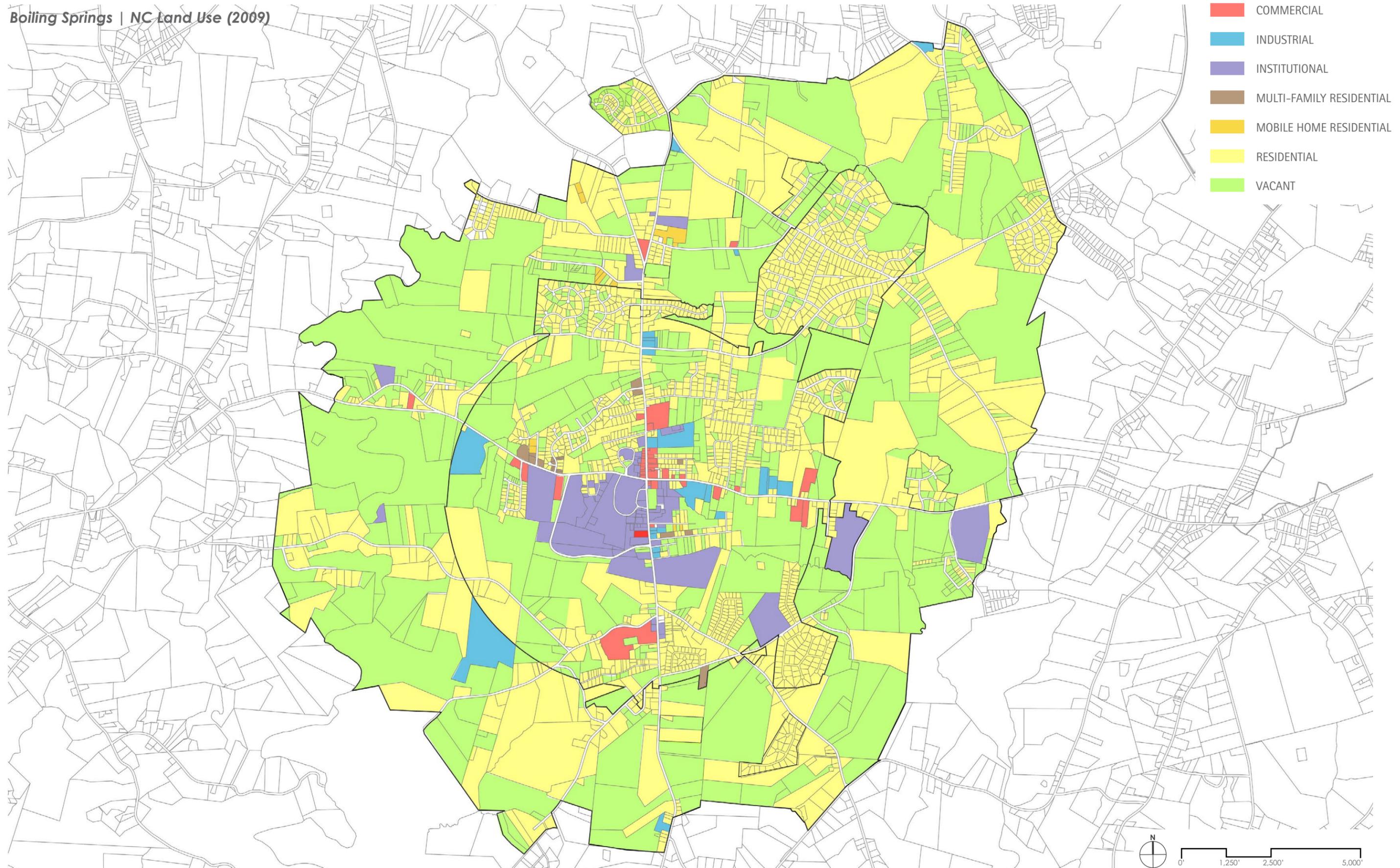
The information provided throughout this chapter is a synthesis of the preliminary analysis documentation developed throughout the Downtown Master Plan process.



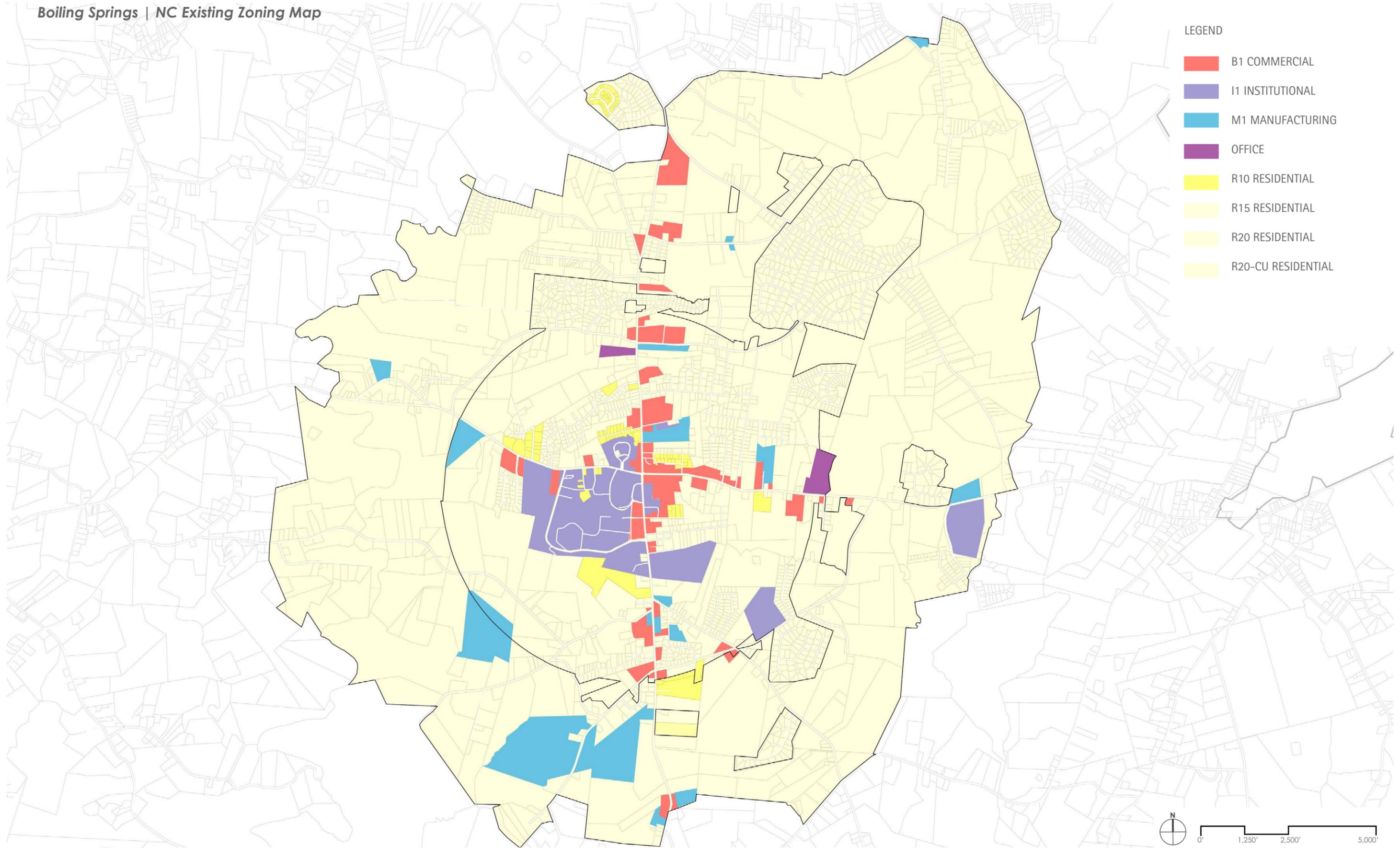








Boiling Springs | NC Existing Zoning Map

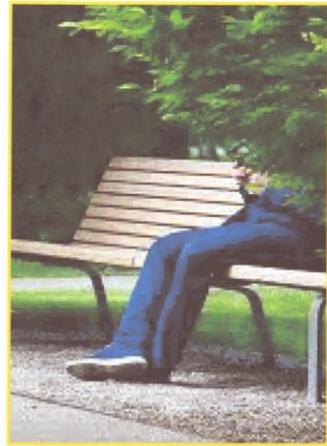


LEGEND

- B1 COMMERCIAL
- I1 INSTITUTIONAL
- M1 MANUFACTURING
- OFFICE
- R10 RESIDENTIAL
- R15 RESIDENTIAL
- R20 RESIDENTIAL
- R20-CU RESIDENTIAL

# STREETSCAPE + GREENWAY AMENITY PREFERENCE

Vote on the top THREE elements you would like to see integrated into downtown Boiling Springs using the provided stickers.



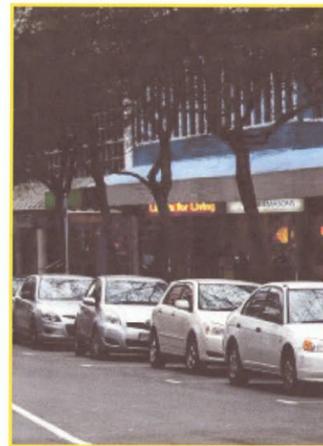
**BENCHES**

3 red stickers



**BIKE RACKS**

2 red stickers



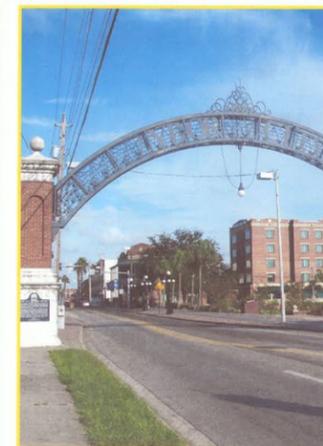
**ON-STREET PARKING**

3 red stickers



**BIKE FIX-IT STATIONS**

0 red stickers



**GATEWAYS**

2 red stickers



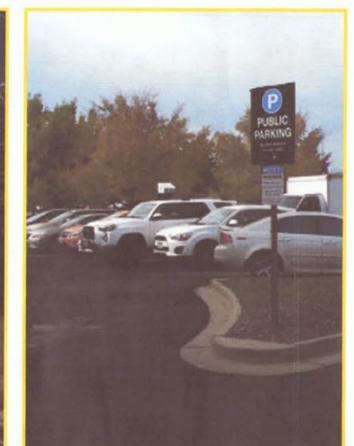
**SIDEPATH/GREENWAY**

11 red stickers



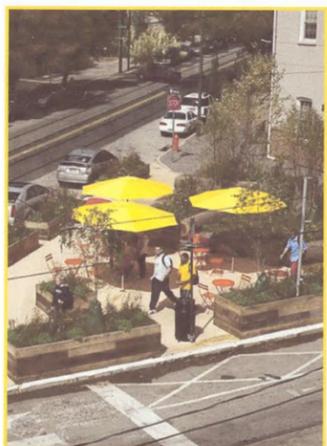
**LIGHTING**

5 red stickers



**PUBLIC PARKING LOT**

10 red stickers



**GATHERING SPACE/ PLAZA**

13 red stickers



**SHADE TREES + LANDSCAPING**

18 red stickers



**SWINGS**

2 red stickers



**TRASH RECEPTACLES**

0 red stickers



**WATER FOUNTAINS**

2 red stickers



**WAYFINDING SIGNAGE**

2 red stickers

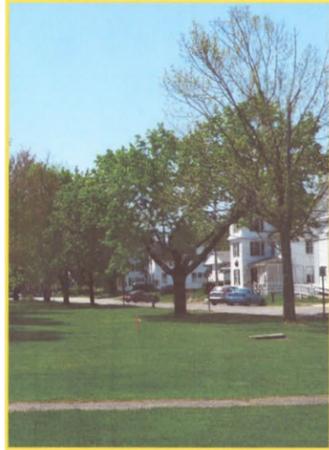
**OTHERS?**

Senior living like Crystal Springs, Shelby

1 red sticker

# TOWN AMENITY PREFERENCES

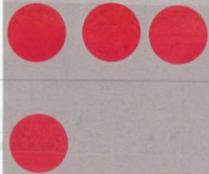
Vote on the top THREE elements you would like to see integrated into downtown Boiling Springs using the provided stickers.



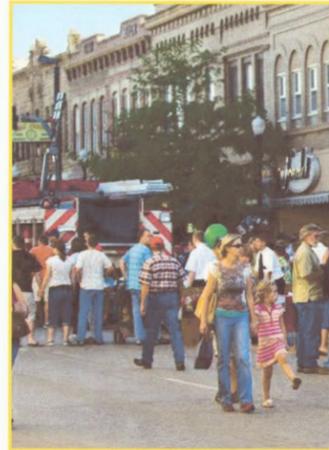
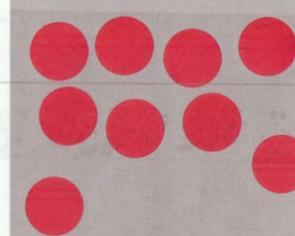
TOWN GREEN



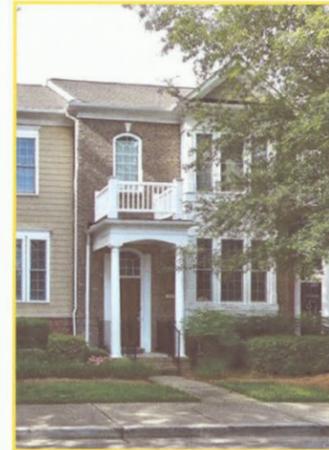
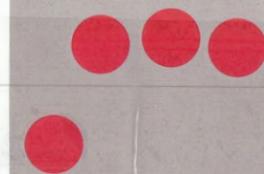
OPEN SPACE



FARMER'S MARKET



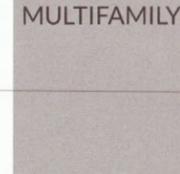
STREET FESTIVALS



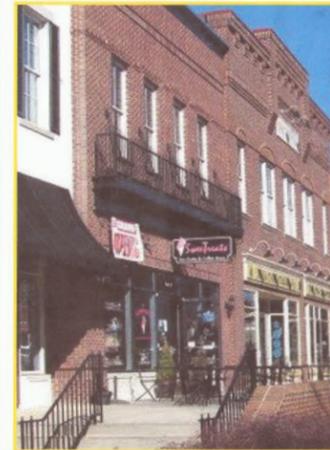
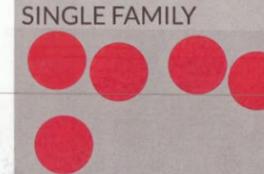
HIGH DENSITY HOUSING TOWN HOME



HIGH DENSITY HOUSING MULTIFAMILY



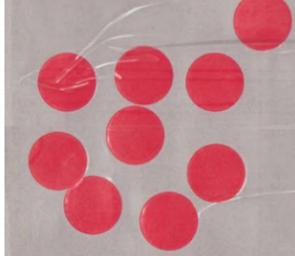
LOW DENSITY HOUSING SINGLE FAMILY



MIXED- USE RETAIL + HOUSING



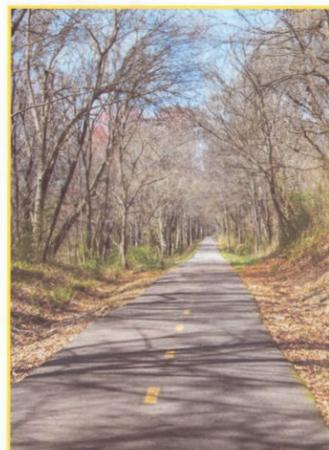
OUTDOOR DINING



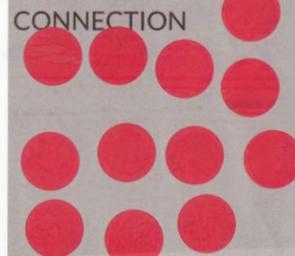
COMMERCIAL DEVELOPMENT - BOUTIQUE SHOPS



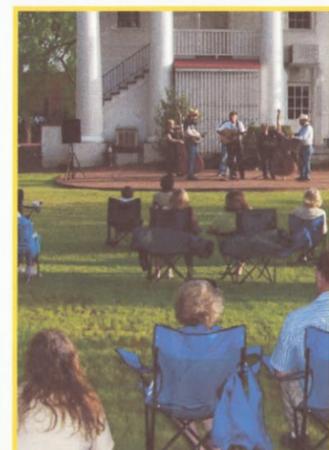
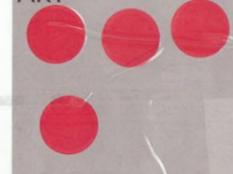
COMMERCIAL DEVELOPMENT - SHOPS



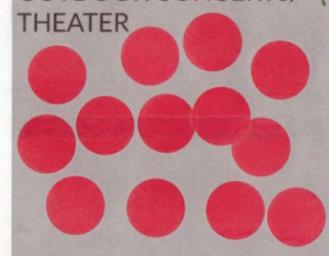
GREENWAY CONNECTION



ART

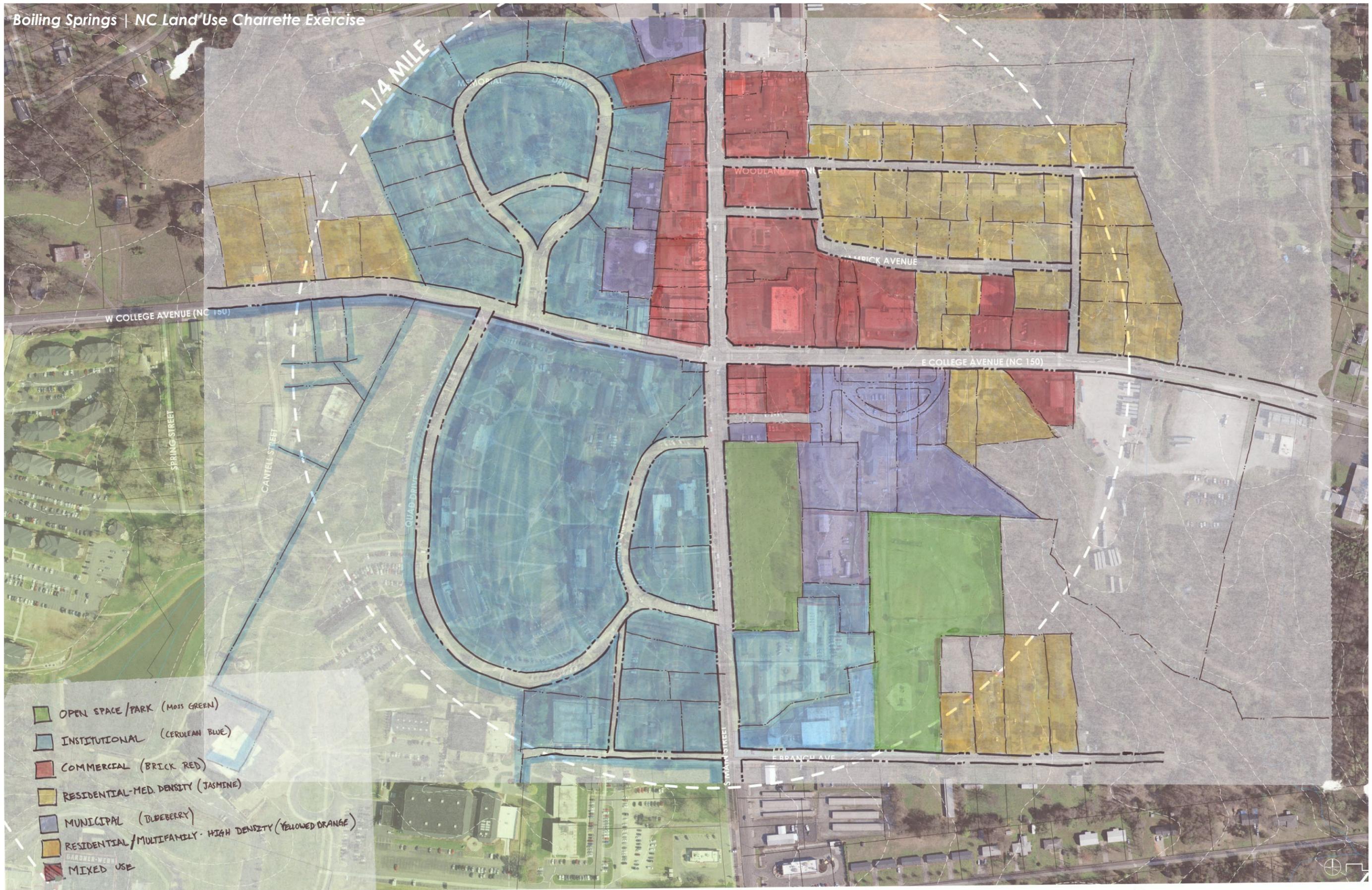


OUTDOOR CONCERTS/ THEATER



OTHERS?

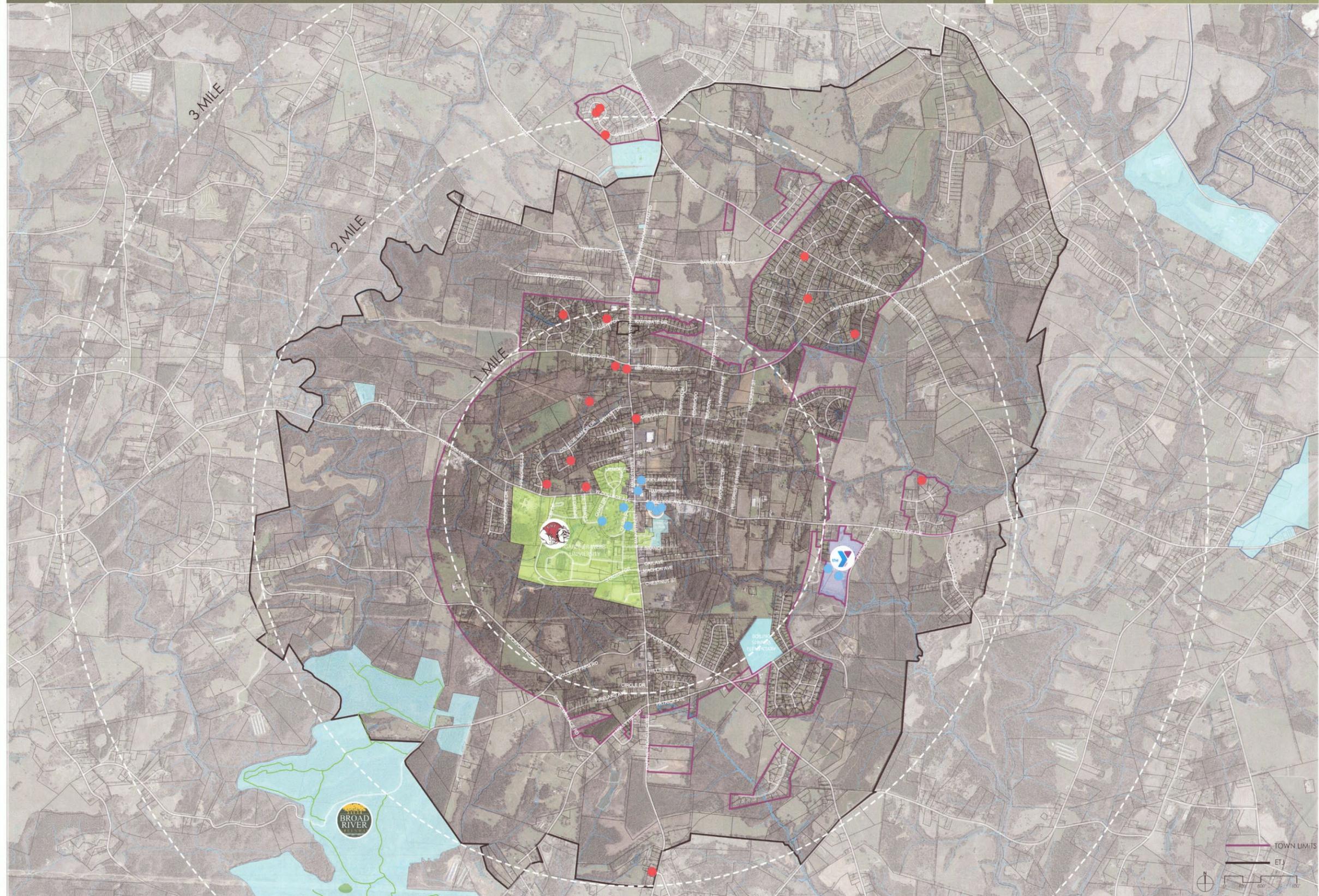
*New grocery store* ●  
*Connectors to annexed neighborhoods*





# WHERE DO YOU LIVE AND WORK?

PLACE A DOT ON THE MAP  
LIVE ●●●●● WORK ●●●●●







**BOILING SPRINGS COMMUNITY PARK**  
EXHIBIT A - CONCEPTUAL LAYOUT





